

## AGENDA FOR THE



### CITY OF PINOLE PLANNING COMMISSION SPECIAL MEETING

**Monday, June 13, 2016**

**7:00 P.M.**

**City Council Chambers, 2131 Pear Street, Pinole, CA 94564**

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In compliance with the Americans with Disabilities Act of 1990, if you need special assistance to participate in a City meeting or you need a copy of the agenda, or the agenda packet in an appropriate alternative format, please contact the Development Services Department at (510) 724-9014. Notification of at least 48 hours prior to the meeting or time when services are needed will assist the City staff in assuring that reasonable arrangements can be made to provide accessibility to the meeting or service.

Assistant listening devices are available at this meeting. Ask staff if you desire to use this device.

#### **CONSENT CALENDAR:**

All matters listed under the Consent Calendar are considered to be routine and non-controversial. These items will be enacted by one motion and without discussion. If, however, any interested party or Commissioner(s) wishes to discuss a consent item, it will be removed from the Consent Calendar and taken up in order after the last item under New Business.

#### **PROCEDURE FOR CONSIDERING AN AGENDA ITEM:**

At the beginning of an item, the Chair will read the description of that item as stated on the Agenda. The City Staff will then give a brief presentation of the proposed project. The Commission may then ask Staff questions about the item.

For those items listed as Public Hearings, the Chair will open the public hearing and ask the applicant if they wish to make a presentation. Those persons in favor of the project will then be given an opportunity to speak followed by those who are opposed to the project. The applicant will then be given an opportunity for rebuttal.

The Public Hearing will then be closed and the Commission may discuss the item amongst themselves and ask questions of Staff. The Commission will then vote to approve, deny, approve in a modified form, or continue the matter to a later date for a decision. The Chair will announce the Commission's decision and advise the audience of the appeal procedure.

**Note: No Public Hearings will begin after 11:00 p.m. Items still remaining on the agenda after 11:00 p.m. will be held over to the next meeting.**

#### **CITIZEN PARTICIPATION:**

Persons wishing to speak on an item listed on the Agenda may do so when the Chair asks for comments in favor of or in opposition to the item under consideration. After all of those persons wishing to speak have done so, the hearing will be closed and the matter will be discussed amongst the Commission prior to rendering a decision.

Prior to speaking on an item, you must fill out one of the speaker cards (available at the back of the Council Chambers) and hand it to the Secretary. If a number of persons wish to speak on an item, the Chair may limit each speaker to a set time period in which to address the Commission.

Any person may appeal an action of the Planning Commission or of the Planning Manager by filing an appeal with the City Clerk, in writing, within ten (10) days of such action. Following a Public Hearing, the City Council may act to confirm, modify or reverse the action of the Planning Commission or Planning Manager. The cost to appeal a decision is \$803.

**Note:** If you challenge a decision of the Commission regarding a project in court, you may be limited to raising only those issues you or someone else raised at the public hearing or in writing delivered to the City of Pinole at, or prior to, the public hearing.

**A. CALL TO ORDER**

**B. PLEDGE OF ALLEGIANCE AND ROLL CALL**

**C. CITIZENS TO BE HEARD:**

The public may address the Planning Commission on items that are within its jurisdiction and not otherwise listed on the agenda. Planning Commissioners may discuss the matter brought to their attention, but by State law (Ralph M. Brown Act), action must be deferred to a future meeting. Time allowed: five (5) minutes each.

**D. CONSENT CALENDAR:**

1. **Planning Commission Meeting Minutes from May 23, 2016**

**E. PUBLIC HEARINGS:**

1. **Design Review (DR 15-10) and Conditional Use Permit (CUP15-02): Gateway Medical Center.**

**Request:** Consideration of a design review and conditional use permit request to construct a new approximately 9,182 square foot two-story medical office building with 10 new automobile parking spaces and accompanying improvements for an ambulatory surgical center on an approximately 26,090 square foot parcel with 21 existing automobile parking spaces and allow a proposed six auto parking space reduction in the normally required number of parking spaces.

**Applicant:** AGAPE, LLC  
1214 McDonald Drive  
Pinole, CA 94564

**Location:** Southeast corner of the intersection of Pinole Valley Road and Henry Avenue, APN: 401-211-033

**Project Planner:** Winston Rhodes

**F. OLD BUSINESS: None**

G. **NEW BUSINESS:** None

H. **CITY PLANNER'S/COMMISSIONER'S REPORT:**

I. **COMMUNICATIONS:**

1. Copy of East Bay Regional Park District Informational Presentation Made to City Council on June 7, 2016

J. **NEXT MEETING:**

Planning Commission Regular Meeting, June 27, 2016 at 7:00PM

K. **ADJOURNMENT**

**POSTED: June 9, 2016**



**Winston Rhodes, AICP**  
**Planning Manager**

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**DRAFT**

**MINUTES OF THE  
PINOLE PLANNING COMMISSION**

**May 23, 2016**

**A. CALL TO ORDER: 8:33 P.M.**

**B. PLEDGE OF ALLEGIANCE AND ROLL CALL:**

Commissioners Present: Brooks, Hartley, Martinez-Rubin\*, Tave, Wong,  
Thompson, Chairperson Kurrent

\*Commissioner Martinez-Rubin arrived after Roll Call

Commissioners Absent: None

Staff Present: Winston Rhodes, Planning Manager

**C. CITIZENS TO BE HEARD:**

There were no citizens to be heard.

**D. CONSENT CALENDAR:**

**1. Planning Commission Meeting Minutes from April 25, 2016**

**MOTION** to approve the Planning Commission Meeting Minutes of April 25, 2016,  
as submitted.

**MOTION: Thompson      SECONDED: Martinez-Rubin      APPROVED: 7-0**

**E. PUBLIC HEARINGS: None**

**F. OLD BUSINESS:**

**1. Selection of Development Review Subcommittee Members for 2016-2017**

Planning Manager Winston Rhodes presented the staff report dated May 23, 2016.

The Planning Commission discussed the purpose of the Development Review Subcommittee at length; encouraged the continued process of workshops for complex projects prior to a scheduled public hearing; recommended the selection of

1 three members, one of which would be a permanent member and an alternate; the  
2 second and third members to rotate membership every year to allow more  
3 members to serve regularly; recommended the Subcommittee be comprised of the  
4 Chair plus a Planning Commissioner and an alternate; with recognition the  
5 Subcommittee must comply with the regulations of the Brown Act.  
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7 **MOTION** to form a Development Review Subcommittee consisting of the Chair,  
8 one Planning Commissioner, and one Alternate for a period of one (1) year.  
9

10 **MOTION: Brooks**                      **SECONDED: Hartley**                      **APPROVED: 6-0-1**  
11 **ABSTAIN: Thompson**  
12

13 **MOTION** to appoint Chair Kurrent and Commissioner Wong as the Primary  
14 Members, and Commissioner Hartley as the Alternate to the Development Review  
15 Subcommittee for 2016-2017.  
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17 **MOTION: Thompson**                      **SECONDED: Tave**                      **APPROVED: 7-0**  
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19 **G. NEW BUSINESS:**  
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21 **1. General Plan Conformity Review for Acceptance of Deeded Areas of**  
22 **Property from the Contra Costa County Flood Control District to the**  
23 **City of Pinole**  
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25 Development Services Director/City Engineer Tamara Miller presented the staff  
26 report dated May 23, 2016.  
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28 **PUBLIC COMMENTS OPENED**  
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30 There were no comments from the public.  
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32 **PUBLIC COMMENTS CLOSED**  
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34 **MOTION** to adopt Resolution 16-04, a Resolution of the City of Pinole Planning  
35 Commission that the Proposed Acquisition of Approximately 3,360 Square Feet of  
36 Property Located Adjacent to the Pinole Hercules Water Pollution Control Plant by  
37 Grant Deed and Quitclaim Deed from the Contra Costa County Flood Control  
38 District Conforms to the City's General Plan.  
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40 **MOTION: Hartley**                      **SECONDED: Brooks**                      **APPROVED: 7-0**  
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42 **H. CITY PLANNER'S / COMMISSIONERS' REPORT:**  
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44 Mr. Rhodes reported that the East Bay Regional Park District (EBRPD) would  
45 make a presentation to the City Council on June 7 related to plans for a trail  
46 extension from Bayfront Park all the way to the property on the opposite side of

1 the railroad tracks, to a trail where Pinole Shores Drive terminates and a EBRPD  
2 parking lot is located. The item would be submitted to the Planning Commission  
3 for design review. In addition, special meetings of the Planning Commission had  
4 been scheduled for June 13 and July 11.

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6 Commissioner Brooks reported that the property located at 3318 Pinole Valley  
7 Road had construction occurring between the property line and retaining wall,  
8 although the area was intended to be green space. He asked staff to review the  
9 matter.

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11 I. **COMMUNICATIONS:** None

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13 J. **NEXT MEETING:**

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15 The next meeting of the Planning Commission will be a Special Meeting on  
16 Monday, June 13, 2016 at 7:00 P.M.

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18 K. **ADJOURNMENT:** 9:42 P.M

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20 Transcribed by:

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22  
23 Anita L. Tucci-Smith  
24 Transcriber  
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# Memorandum

**TO:** PLANNING COMMISSION MEMBERS  
**FROM:** WINSTON RHODES, AICP, PLANNING MANAGER  
**SUBJECT:** Gateway Medical Center  
**DATE:** April 25, 2016

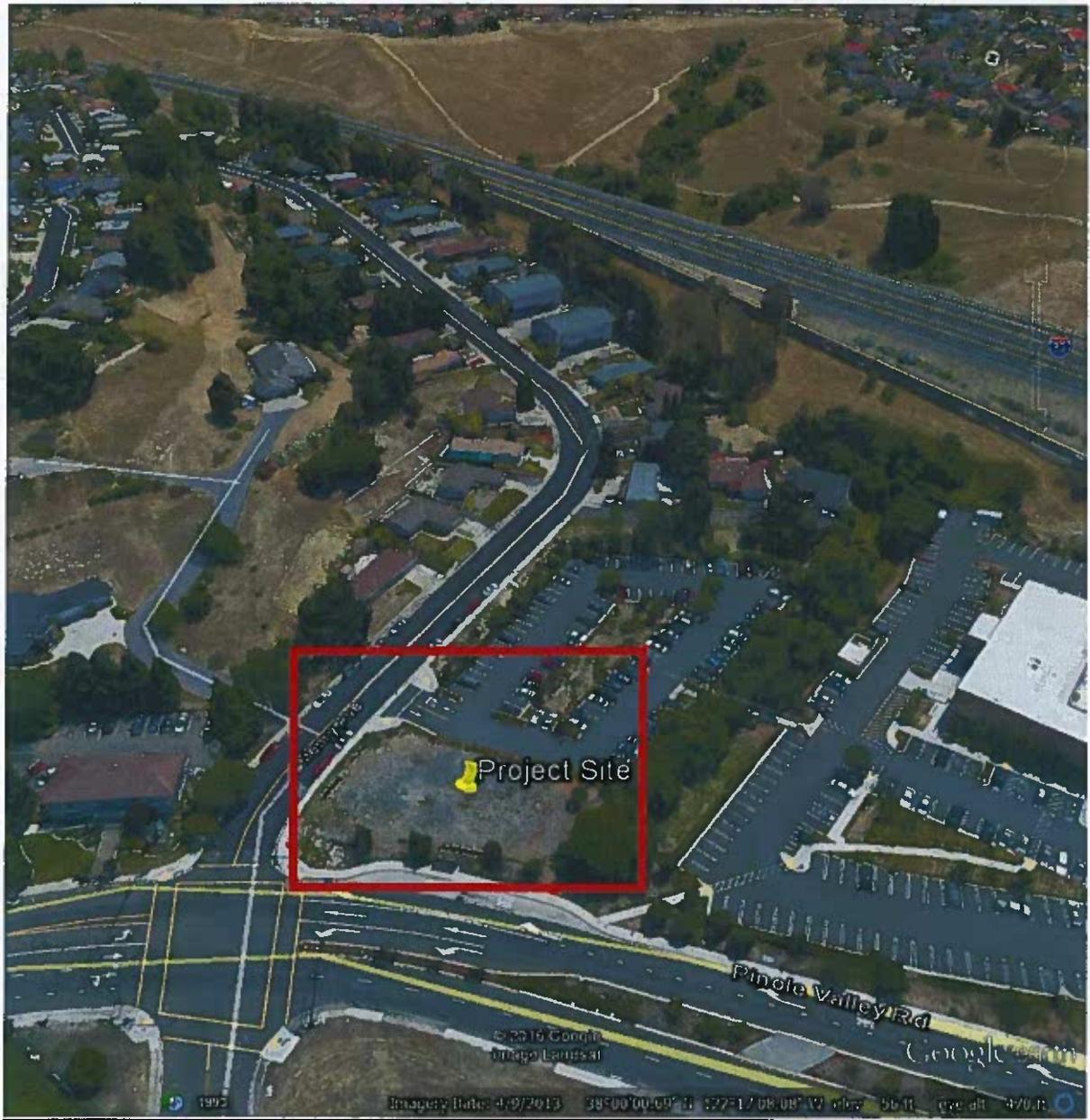
<b>File: Design Review (DR 15-10) / Conditional Use Permits (CUP15-02)</b>	
LOCATION: Southeast corner of Pinole Valley Road and Henry Avenue	
APN: 401-021-033	
ZONING: Office Professional Mixed Use (OPMU)	
GP LU: Service Sub Area (SSA)	
<b>Applicant:</b> Agape LLC 1214 McDonald Drive Pinole, CA 94564	<b>Property Owner:</b> City of Pinole

## REQUEST

Consideration of a design review and conditional use permit request to construct a new approximately 9,182 square foot two-story medical office building with 10 new automobile parking spaces and accompanying improvements for an ambulatory surgical center on an approximately 26,090 square foot parcel with 21 existing automobile parking spaces and allow a proposed six auto parking space reduction in the normally required number of parking spaces.

## SITE LOCATION

The proposed Gateway Medical Center is located southeast of the intersection of Pinole Valley Road and Henry Avenue (APN: 401-211-033).



**Figure 1. Site Location**

<b>Direction from Project Site</b>	<b>Land Use</b>
North	Henry Avenue, Office, and Collins Elementary School
West	Pinole Valley Road and Approved Retail
South	Riparian Area, Medical Office and Approved Retail
East	Kaiser Medical Clinic and Interstate 80

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## **PROJECT DESCRIPTION**

The project involves the proposed construction of a new two-story medical office building on a partially developed corner property southeast of the intersection of Pinole Valley Road and Henry Avenue. The proposed building is approximately 9,182 square feet and includes ten new parking spaces to accompany 21 existing parking spaces.

The City's Municipal Code authorizes the Planning Commission to take final action on these land use requests.

## **BACKGROUND**

The applicant desires to develop the new medical office building on property currently owned by the City of Pinole and purchase the property from the City of Pinole. The sale of the property requires State approval since the property was originally purchased by the City's Redevelopment Agency. The applicant has a long-standing medical practice within the City and wishes to stay in Pinole and develop a modern facility to better serve existing and future patients.

The project site was previously reviewed for development in 2005 in conjunction with the Kaiser Medical Center project and was planned for an approximately 5,000 square foot one-story medical office building. During the development of the Kaiser Medical Center south and east of the property, parking lot improvements were constructed on the project site in conjunction with a Kaiser employee parking lot accessed from Henry Avenue. In 2010, the City updated the General Plan, adopted the Three Corridor Specific Plan and updated the Zoning Code. The project site is within the Pinole Valley Road corridor included within the Three Corridors Specific Plan. The General Plan land use designation is Service Sub-area (SSA) and the Zoning for the property Office Professional Mixed Use (OPMU). The proposed surgery center is a permitted use in the OPMU Zoning District.

In August 2015, the applicant submitted the initial design for the proposed development. It was a two-story design totaling approximately 9,939 square feet and included a total of 29 parking spaces. This design was discussed with the City Council, as the property owner, in an informational presentation. Feedback from the City Council included reducing the size and bulk of the building, adding on-site parking, and changing the exterior colors and modifying the exterior finishes of the building. In response, the building height was also reduced by six feet, the square footage was decreased by approximately 8%, two more parking spaces were added, and colors were modified to better complement building colors of the Kaiser medical center and the approved Starbucks and Sprouts buildings.

In December 2015, the applicant submitted a development application for the Gateway Medical Center to the City. The Planning Commission Development Review Subcommittee met on January 21, 2016 to discuss the project site plan, architecture and landscape plan. The Development Review Subcommittee thought that the site plan was well-composed and that the architecture while very modern was very distinctive and was compatible with existing and planned new development in this "Gateway" portion of Pinole Valley Road. The Subcommittee did request further photo-simulation information to provide more information about the proposed height and bulk of the medical office building.

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A Planning Commission workshop was held on April 25, 2016. Commission members provided feedback about the project design summarized below;

- Thanked the applicant for the preservation of the Faria Ranch historic monument. (Martinez-Rubin)
- Supported the inclusion of time restrictions for most convenient on-site parking spaces to encourage maximum use by patients and visitors throughout day; sought better renderings to show the location of the rooftop mechanical equipment and how it would be screened from view; since the building had been reduced in height it now appeared too flat, recommended consideration of more articulation; and supported the building height at 32 feet rather. (Wong)
- Requested an analysis of Kaiser's loss of 21 parking spaces and further discussions with Kaiser about the utilization of all of its parking spaces, not just the employee parking and encouraged the use of public transportation in the project vicinity. (Kurrent)
- Suggested the building design was too modern, its character was not appropriate for Pinole given the proximity to Old Town. (Thompson)
- Preferred tandem parking be designated for employees only ; suggested consideration of outside lighted display cases for artwork; and encouraged staff to raise the broader traffic concerns and issues with the Traffic and Pedestrian Safety Committee (TAPS), at which time some solutions could be disc. (Tave)

## **ANALYSIS**

### ***General Plan Conformance***

The project site is designated as Service Sub Area (SSA) in the City's General Plan. Applicable policy language from the General Plan that supports the development of the project site for the proposed medical office use includes:

#### **Community Character Element**

**Policy CC.1.1** All new development and redevelopment shall adhere to the basic principles of high-quality urban design and architecture including, but not limited to, human- scaled design, pedestrian orientation, and interconnectivity of street layout, siting buildings to highlight important intersections, entryways, focal points and landmarks.

**Policy CC.1.3** To enhance a sense of arrival and create a strong appealing image that promotes community identity, the City shall develop community entry features at key gateways or city entries along Interstate 80. Entryways shall incorporate landscaping, trees, structural architectural elements, signage and public art.

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Policy CC.1.5 Encourage project compatibility, interdependence and support with neighboring uses, especially between commercial and mixed-use centers and the surrounding residential neighborhoods. Uses should relate to one another with pedestrian connections, transit options, shared parking, landscaping, public spaces, and the orientation and design of buildings.

**Growth Management Element**

GOAL GM.4 Compact Development and Service Areas. Encourage infill and redevelopment in areas that are already served by utilities, infrastructure and public services.

**Land Use and Economic Development Element**

Policy LU.3.3 Require design review of commercial and industrial projects to ensure compatibility with adjacent or nearby land uses, including intensity, access, internal circulation, visual characteristics, noise, odors, fire hazards, vibrations, smoke, discharge of wastes and nighttime lighting.

Policy LU.4.1 Ensure all new development, renovation or remodeling preserves and strengthens Pinole's residential neighborhoods by requiring projects to be harmoniously designed and integrated with the existing neighborhood.

Policy LU.7.1 Provide sufficient land for commercial and industrial uses to allow for development that provides basic goods and services to Pinole residents.

Policy LU.7.2 Provide for economic development which (1) maintains the City's ability to finance services and the construction and maintenance of public improvements; (2) offers local employment opportunities for Pinole residents to reduce inter-city commuting; and (3) assures the availability and diversity of resident-serving goods and services.

Policy LU.7.5 Ensure that new and existing developments can be adequately served by municipal services and facilities in accordance with City standards. New projects which require construction or expansion of public improvements shall pay their fair share of the costs necessary to improve or expand infrastructure to serve them, including street improvements, parks, water storage tanks, sewer and water service, and other public services.

**Circulation Element**

Policy CE.1.1 Encourage strategic growth that concentrates future development along Pinole's three primary transit corridors (San Pablo Avenue, Appian Way and Pinole Valley Road).

Policy CE.1.4 Encourage maximum utilization of the existing public transit system and alternate modes of transportation in Pinole.

Policy CE.8.1 Require development to provide pedestrian walkways that are safe, interconnected and accessible by all members of the community.

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Policy CE.8.2            Encourage the community to take advantage of Pinole's pedestrian facilities and recreational opportunities and increase non-motorized modes of transportation.

Policy CE.8.4            Encourage the location of basic shopping and services within walkable distances to residential areas.

***Three Corridors Specific Plan Conformance***

The project site is included within the "Three Corridors Specific Plan". As defined on page 5.0-21 of the Land Use and Economic Development Element of the General Plan, future development and redevelopment along the designated corridors – Pinole Valley Road, San Pablo Avenue and Appian Way – is intended to create "an opportunity to concentrate future development, improve transit service by encouraging transit-oriented development projects, create more mixed-use development, and encourage redevelopment and substantial new economic investment over time." More specifically, the focus of the Pinole Valley Way Corridor is that it will be a pedestrian-friendly corridor where each use has its own distinct character, walking from one use to another will be an interesting experience. The Service Sub-Area is intended to serve as the gateway into Pinole and serve as a transition area between the urban core and the existing single-family residential areas in Old Town and outside the Specific Plan boundaries. The subject property is designated Office Professional Mixed Use (OPMU). The purpose of the OPMU zoning district is to allow development of larger office buildings and business parks with supporting retail and service uses. The predominant use is office, but commercial uses may be integrated into office buildings or located horizontally in freestanding buildings. At least 51 percent of the total usable floor area should be office uses. The proposed project helps to implement the Specific Plan land use policies below.

LAND USE POLICY 5            Support existing viable uses while encouraging a new mix of uses.

LAND USE POLICY 6            Actively promote the "revitalization" of underutilized land.

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LAND USE POLICY 8            Encourage the development of mixed use office buildings in proximity to existing transit stops.

**Zoning Conformance**

**Design Review**

Section 17.12.150 (A) of the Zoning Ordinance states that “comprehensive design review is intended to . . . encourage development in keeping with the desired character of the city, and to ensure physical and functional compatibility between uses.” The proposed Gateway Medical Center will be a two story, flat-roofed structure featuring a mix of materials and architectural details as well as high degree of articulation on all four sides. The project site is partially developed and includes 21 existing parking spaces abutting the existing Kaiser employee parking lot to the east. To the south is a riparian corridor that features a day-lighted tributary of Pinole Creek near existing parking spaces. The applicant has designed the proposed building to provide a larger setback from the top of the creek bank than now exists between current parking area and the top of the creek bank. Cross-section information has been provided indicating a buffer from approximately 18 feet to approximately 30 feet from the proposed building site.

West of the site is Pinole Valley Road and an existing bus stop with rider shelter and bus turnout and young trees behind the sidewalk. The applicant is proposing to retain all but one of these young trees.

North of the project site is Henry Avenue and on-street parking spaces. There is a petroleum pipeline easement with active petroleum pipelines that crosses the northwest corner of the project site. The applicant has located the proposed building entirely outside the easement. A circular outdoor patio area is proposed to encroach within the easement area. A draft condition of approval has been prepared to require reconfiguration of the outdoor patio area in order to avoid the easement. Additionally a draft condition of approval has been prepared requiring close coordination with the pipeline easement holders during the development process.

The entrance to the building is on the east elevation facing the parking lot. It features a glass wall and covered entrance flanked by metal poles extending from the ground surface through the covered entrance to the roof plate of the building. The east elevation includes a second floor that protrudes out towards the parking area and provides a covered walkway and overhang above the seven parking spaces closest to the building. The south end of the building features an exterior stairway. The west side of the building features a very prominent glass wall extending from the ground to the second floor providing natural light to the lobby area and lounge. The applicant has proposed an outdoor seating area opposite the southeast corner of the intersection of Pinole Valley Road and Henry Avenue.

Section 17.12.150 (H) of the Zoning Ordinance identifies four criteria that must be considered by the Planning Commission “in conducting comprehensive design review.” They are as follows and are addressed specifically regarding the proposed Gateway Medical Center building:

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Considerations relating to site layout, the orientation and location of building, signs, other structures, open spaces, landscaping, and other development features in relation to the physical characteristics, zoning, and land use of the site and surrounding properties.

*The proposed Medical Center colors and landscaping are consistent with existing commercial development along this portion of Pinole Valley Road. The structure and materials are distinctive and modern in appearance intended to provide a sleek design. The proposed materials include use of stone, stucco, glass, and colored cement panels with an acrylic coating. The building height is approximately 30 feet. Multi-story buildings are currently located north of the project site at the northeast corner of Henry Avenue and San Pablo Avenue and south of the project site (Kaiser medical center). The proposed medical center height is also shorter than the approved Sprouts building on the west side of Pinole Valley Road opposite the project site. Similar to Kaiser it is a modern departure from architecture within the Old Town. It is both a unique and distinctive design intended to be compatible with but different from any of the surrounding buildings by utilizing some of the similar materials (e.g. stucco and glass). The building is located close to Pinole Valley Road similar to the two buildings approved west of Pinole Valley Road within the Gateway Shopping Center and includes an outdoor patio to provide a more pedestrian-oriented appearance. The proposed building is appropriately located on the site to avoid the existing petroleum pipeline easement, yet be conveniently accessible by transit vehicles and pedestrians from the surrounding area.*

Considerations relating to traffic, safety, and traffic congestion, including the effect of the development plan on traffic conditions on abutting streets, the layout of the site with respect to locations and dimensions of vehicular and pedestrian entrances, exits, driveways, and walkways, the adequacy of off-street parking facilities to prevent traffic congestion, and the circulation patterns within the boundaries of the development.

*The project's traffic impacts were most recently evaluated in the Gateway Shopping Center Mitigated Negative Declaration which assumed the development of a 10,000 square foot office building. The project would not cause any intersections in the study area to exceed City, County, or Caltrans standards and vehicular traffic mitigations would not be required. In addition, the proposed project is consistent with what has been anticipated for the site by the City since General Plan Update in 2010. As such, buildout of the site has already been assumed in all cumulative build-out traffic forecasts that have been used in the design of freeway facilities in the area. Accordingly, the proposed project would not cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system or that would exceed an established LOS standard, and no impact would occur.*

*The proposed project would retain the number of existing driveways on Henry Avenue. The single driveway would continue to be a two-way and located along north edge of the property and away from the intersection of Henry Avenue and Pinole Valley Road to avoid interfering with efficient operation of that intersection. There is no direct automobile access to the site from Pinole Valley Road. Due to the size of the property two-way traffic is limited to a single drive aisle closest to the east side of the building. The 31 proposed spaces of off-street auto parking are six less than the required 37 spaces based on a parking standard of one space per 250 square feet of medical office space. However, the Zoning code does allow for reductions with a conditional use permit as discussed further below.*

*As discussed previously in this report, the project design conforms to the applicable policies of the*

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*Pinole General Plan, the Three Corridors Specific Plan and the applicable provisions of the Zoning Ordinance.*

Considerations relating to the availability of city services, including, but not limited to, water, sewer, drainage, police and fire, and whether such services are adequate based upon city standards.

*The proposed project would connect to the City's existing utility lines in the area in order to provide service to the site.*

#### *Stormwater*

*Runoff from the existing paved portion of the project site, consisting of striped parking spaces, currently sheet flows to the on-site rock swale that terminates in an on-site drain inlet. The drain inlet connects to an 18-inch storm drain pipe running north-south on the project site, and connecting to the 24-inch storm drain in Pinole Valley Road. Runoff from the existing on-site parking area would continue to sheet flow into the rock swale, as it does today (see **Error! Reference source not found.**). The majority of new parking would be covered by the building's roof, the runoff from which would be routed to flow-through planters.*

*The proposed project includes two treatment areas in the form of flow-through planters. All roof leaders would drain into the flow-through planters to help slow and treat stormwater runoff from the roof's impervious surfaces. The northerly infiltration planter will ultimately convey flows to the existing catch basin at the southeast corner of the Pinole Valley Road/Henry Avenue intersection via a new on-site 6-inch storm drain pipe. The southwesterly infiltration planter will connect to the 18-inch on-site storm drain pipe via a new on-site 6-inch storm drain pipe.*

#### *Sewer*

*The City of Pinole is responsible for the collection and treatment of wastewater for this site. The proposed project would connect to an existing sewer line in Henry Avenue. An existing lateral connection would be removed and replaced with a four-inch lateral. The wastewater would then be treated at the Pinole-Hercules Water Pollution Control Plant.*

#### **Parking - Conditional Use Permit**

The City of Pinole Municipal Code Chapter 17.48 requires one parking space per 250 square feet of gross floor area. The project's proposed square footage of 9,182 would require 37 parking spaces. The project plans include 31 parking spaces, which consists of 21 existing parking spaces and 10 new spaces along the front of the building. The proposed parking total is six less than that which is required by the City of Pinole Municipal Code. Consequently, the proposed project includes a request for approval of a Conditional Use Permit to allow for a reduction in the number of parking spaces required. Section 17.48.060 requires the Planning Commission to make several findings before approving a conditional use permit for reduction in parking.

Criteria for approval. The Planning Commission may only grant a conditional use permit for reduced parking if it finds that the project meets all of the conditional use permit criteria in Section 17.12.140 (Conditional Use Permits) and that three (3) or more of the circumstances listed below are true.

a. The use will be adequately served by the proposed parking due to the nature of the proposed operation; proximity to frequent transit service; transportation characteristics of persons residing, working, or visiting the site; or because the applicant has undertaken a travel demand management program that will reduce parking demand at the site.

b. Parking demand generated by the project will not exceed the capacity of or have a detrimental impact on the supply of on-street parking in the surrounding area.

c. The site plan is consistent with the objectives of the zoning district and incorporates features such as unobtrusive off-street parking placed below the ground level of the project with commercial uses above or enclosed parking on the ground floor.

d. The applicant has provided on-site parking for car share vehicles via a recorded written agreement between the landowner and the city that runs with the land. Agreement shall provide for proof of a perpetual agreement with a car share agency to provide at least one (1) car share vehicle on-site.

Applicant proposed measures to make the needed findings are included in the attached project description (see Attachment A). A Parking Study has been prepared for the project by Abrams Associates, demonstrating that three of these findings (a, b, and d) can be made (see Attachment B).

The draft resolution (see Attachment D) includes project conditions of approval requiring:

- Provision of free bus passes for all medical center employees;
- 90-minute time limits for the most convenient parking spaces;
- Inclusion of a patient pickup and drop off parking space;
- A reserved space for ride share vehicles;
- Employees who drive to park in the least convenient on-site spaces;
- On site bicycle parking outside near the front building entrance as well as secure bicycle parking inside the medical center; and
- Potential reconsideration of the use permit if the project is intensified or altered in such a manner as to increase demand for parking .

### ***Environmental Review***

Section 21084 of the Public Resources Code requires the California Environmental Quality Act (CEQA) Guidelines to include a list of classes of projects which have been determined not to have a significant effect on the environment and which shall, therefore, be exempt from the provisions of CEQA. Two types of exemptions apply to this project.

Class 3 exemptions are allowed for certain small projects including commercial buildings that are less than 10,000 square feet within urbanized areas that are consistent with zoning requirements, that do not involve the use of significant hazardous material, and where all necessary public services and facilities are available.

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Class 32 exemptions apply to infill project sites that have been previously urbanized, when projects are consistent with the General plan policies, consistent with applicable zoning requirements, are located within city limits on sites less than 5 acres in size; and for project sites that have no value as habitat for endangered, rare, or threatened species. Additionally, to be eligible for this exemption approval of the project would not result in significant effects relating to traffic, noise, air quality, or water quality and the site can be adequately served by all required utilities.

The proposed Gateway Medical Center properly qualifies for an exemption under CEQA Guidelines §15303 and 15332 as both a Class 3 and Class 32 exemption, respectively because it is a small structure on a site located in an urban area of the City of Pinole with less than 10,000 square feet of floor area; is consistent with applicable General Plan polices and the existing Office Professional Mixed Use (OPMU) zoning district as conditioned; does not involve the use of significant amounts of hazardous substances; and is adequately served by all necessary public services and facilities. Therefore, the proposed project meets all requirements established by Section 15030 of the CEQA Guidelines, and is exempt from the provisions of CEQA under Class 3.

Additionally, the proposed Gateway Medical Center project is categorically exempt from CEQA because it is an infill development project consistent with the site's current Service Sub-Area General Plan land use designation and the OPMU zoning district; is located within the limits of the City of Pinole; is approximately 0.6-acres in size; and is surrounded by urban uses and has been heavily disturbed by past grading and partial paving. The site is of no value as habitat for endangered, rare, or threatened species and approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. Existing water and sewer lines are located in surrounding roadways, to which the project would connect for service. The site can also be adequately served by police and fire service providers, both of which are located a short distance to the project site. Therefore, the proposed project meets all qualifications of Section 15332 of the CEQA Guidelines, and is exempt from the provisions of CEQA under Class 32.

### **STAFF RECOMMENDATION**

Adopt Draft Resolution 16-05 conditionally approving the Design Review and Conditional Use Permit for the new medical office building after conducting a public hearing and considering information provided.

### **ATTACHMENTS**

Attachment A Project Description and Project Plans Received 6/2/16 and 12/2/15

Attachment B Trip Generation and Parking Analysis dated 6/6/16

Attachment C Correspondence Received since 4/ 25/16

Greg Jacobs letter dated 6/2/16

Kenneth and Martha Wong letter dated 6/3/16

Attachment D Draft Resolution 16-05 including proposed conditions of approval

Attachment E Proposed Color and Material Boards (to be provided at the meeting)

RECEIVED

JUN 02 2016

CITY OF PINOLE  
DEVELOPMENT SERVICES DEPT.

Lic.# 794702

May 16, 2016

**TO:** City of Pinole  
2131 Pear Street  
Pinole, CA 94564

Email: wrhodes@ci.pinole.ca.us

**ATTN:** Mr. Winston Rhodes, AICP

**RE:** Gateway Medical Center  
CUP 15-02 / DR 15-10)

**FROM:** Donna Vingo

Phone: 925-560-9906

Email: [donnav@envisioncdi.com](mailto:donnav@envisioncdi.com)

**PROJECT:** Gateway Medical Center – Pinole (APN 401-211033)  
Pinole Valley Road  
Pinole, CA 94564

Mr. Rhodes,

**Project Description-Gateway Ambulatory Surgical Center**

AGAPE, LLC is proposing to construct a 2-story, 9182 sq. ft., ambulatory surgical center. The facility is specifically designed for an ophthalmology outpatient surgical practice consisting of surgical and medical office space. The facility will be AAAHC (Accreditation Association for Ambulatory Health Care) and OSHPD-III certified to qualify for Medicare programs. The location was carefully selected and is ideal to service the low-income and elder patient population currently residing in the Pinole and surrounding communities. Following the closure of Doctors Medical Center in Pinole this facility will be the only outpatient surgical facility in western Contra Costa County that is a certified outpatient surgical center to provide care for Medicare, Medical and Medicare Advantage HMO patients, fulfilling essential care to the elderly and indigent.

Subsequent to the City Council and Planning Commission Subcommittee comments the project has been reworked to address their comments and concerns.

- The building has been reduced in size by 800 square feet to 9182 square feet to reduce the parking count to a number acceptable to the City Council.
- The roofline has been simplified to be a single plane at 36 feet in height and extended to be more aesthetically pleasing.
- The building design has been simplified and colors modified. The cleaner crisper lines of the building provide a Gateway statement to Pinole Valley Rd, the entrance to Pinole, and still blend with surrounding architecture and materials.

- The Café/food service menu has been scaled back considerably so that only minor preparation, sandwiches etc. with some food being brought in from local vendors. The food and beverages will be available to patients and employees only.
- The area that was café/lounge is now waiting room area in which local artists can display their artwork, further serving the citizens of Pinole and local community.
- An additional patient drop-off area has been added.
- The project provides easy access to a daily needs corridor, major arterial throughway, mass transportation hub and proximity to the downtown area.

The proposed hours of operation are Monday thru Friday 7AM to 7PM, Saturday and Sunday will be for emergencies only. The clinic hours will be 8AM to 5PM and the surgery hours will be 7AM to 5PM Monday thru Friday. Deliveries will be scheduled for non-clinic hours. The number of employees required by Medicare (416.42(b)(2)) and the Board of Nursing and California Department of Public Health (CDPH) for a free standing surgical center is 2 RN's, and 1 physician immediately available. The remainder of the staff required is to be within the standard of care for the specialty. The standard of care for ophthalmology includes 1 surgical technician, 1 administrative staff member and 1 anesthesiologist (immediately available physician) for a total of 5 employees in the surgical suites area and 3 patients. The exam rooms will have 2 staff members and share the administrative staff member, one RN and 3 patients. At maximum capacity, from a functional and regulatory standpoint, with the clinic and surgery center functioning simultaneously the total staffing is 13 employees and 6 patients for a total of 19 parking spaces at maximum use of the facility.

The applicant has incorporated all the City Council, Planning Commission and Planning Commission subcommittee recommendations and suggestions into the project to reduce the size and scope of the building thereby reducing the number of parking spaces that will be required to meet the City of Pinole code of 1 parking space for each 250 square foot of building. Victoria Haas MHA, MBA, a 25 year specialist in OSHPD 3 construction, has provided a letter explaining that the Office of Statewide Health Planning and Development requires multiple rooms/areas that are redundant or never used but required to be in an outpatient surgical center. The OSHPD requires almost 400 sq. ft. of space that is never or largely underutilized but increases the parking requirement to the project. The OSHPD required space increases the parking count for largely empty space. The under- utilized OSHPD sq. ft. consists of;

- The requirement for separate restroom facilities with lockers for each male/female employees, as well as patients.
- The requirement for a separate anesthesia workroom. In an outpatient setting this is not used by anesthesiologist and is now considered an obsolete space. This space will be mostly an empty room.
- The requirement for a separate supervisor office, which is redundant with the front desk. Again this space will most likely be empty.
- The requirement for a space for public access to a phone, when most patients typically have a cell phone.
- The clean utility, dirty utility, and sterile work areas are also required and increase the square footage with significant redundancy as well underutilization.

Incidental areas that add to the square footage but not increased patients or employees are the lobby, exams rooms containing different types of ophthalmic equipment by patient need, waiting rooms, patient food service area etc. totaling almost 2000sq.ft. of space that is not patient related and does not affect the number of parking spaces. This unused square footage adds 8 spaces to the parking count. Ms. Haas determined, judging from her experience and calculations, the estimated number of patients and staff will be 19 if both the clinic and surgical suites were functioning at maximum capacity. The project will be

providing 31 parking spaces, which are 12 parking spaces more than Ms. Haas calculated will be used if the surgery suites and all exam rooms are running at maximum capacity simultaneously.

The applicant is requesting a conditional use permit as the project provides 31 parking spaces which is 6 less than the 37 the is called for in the General Plan for medical office space, at 1 parking space per 250 sq. ft. City Code 17.48.060.B, required parking may be reduced through approval of use permit by the Planning Commission. The criteria are spelled out under this same chapter, and you would have to file for a Conditional Use Permit and meet (3) or more of these circumstances:

- a) *The use will be adequately served by the proposed parking due to the nature of the proposed operation; proximity to frequent transit service; transportation characteristics of persons residing, working, or visiting the site; or because the applicant has undertaken a travel demand management program that will reduce parking demand at the site.*
- b) *Parking demand generated by the project will not exceed the capacity of or have a detrimental impact on the supply of on-street parking in the surrounding area.*
- c) *The site plan is consistent with the objectives of the zoning district and incorporates features such as unobtrusive off-street parking placed below the ground level of the project with commercial uses above or enclosed parking on the ground floor.*
- d) *The applicant has provided on-site parking for car share vehicles via a recorded written agreement between the landowner and the city that runs with the land. Agreement shall provide for proof of a perpetual agreement with a car share agency to provide at least one (1) car share vehicle on site*

Measures taken to fulfill the conditional use permit requirements are as follows:

The use will be adequately served by the proposed parking due to the nature of the proposed operation; Ophthalmology patients in the practice are primarily elderly and on Medicare/Medical. The patient demographics transportation is comprised of 1/3 take public transportation, 1/3 use para-transit and only 1/3 of the patients use a personal vehicle.

- a) A Travel Demand Management Plan will be implemented with this approval, the plan includes;
  - a. All employees will have available vouchers that pay 100% of their public transportation expenses.
  - b. Site is located next to a bus stop which services bus lines 16, 19 and JPX.
  - c. An employee ride share program will be instituted allowing employees to share transportation costs, share commuter lanes and reduce by 1/2 the necessary parking needed on site.
  - d. The applicant bikes to work and other individuals are encouraged to do so with bike racks prominently featured in the front of the building.
- b) No on street parking is being requested.
  - a) The project has 21 parking spaces currently that are being used by illegally parked commuters and Kaiser patients/ employees while the Kaiser parking is consistently one quarter to one third, approximately 40-50 spaces, underutilized. The perception that there is a lack of available parking is due to commuters and Kaiser patients/employees using the project parking and Henry Avenue for parking out of lack of information or lack of parking

enforcement. There is no lack of parking and this project is adding an additional 10 parking spaces so 31 total parking spaces will be provided for the project which greatly overserves the parking needs of this project.

- b) The project will be providing 10 parking spaces in addition to the 21 spaces on site currently for a total of 31 parking spaces.
- c) N/A
- d) The applicant will provide an on-site car share vehicle via an agreement with Getaround. The written agreement between the City and landowner will be in a form acceptable to the city.

The project proposes an in-door/out-door waiting area for patient and family use that will provide basic food and drink services to be provided by local vendors. This area serves three primary purposes:

- Provide a nutritional option for post-operative patients who have been required to be restricted in dietary needs to safely undergo anesthesia;
- Provide a convenient and adjacent snack options for waiting family members without having to leave the premises while their loved one undergoes surgery;
- A display area for local artists (Pinole Artisans) who have few viable display options in the Community, and have been displaced from their current location;

In addition to the waiting area local artist display, public art features have been incorporated into this project in the form of a mural wall facing the Pinole Valley Road street frontage and preservation of the current memorial plaque that currently resides on the corner of Pinole valley Road and Henry Avenue.



## Healthcare Business Solutions

business solutions for outpatient centers

409 Alhambra Ave. Suite 11

San Jose, CA 95128

Office 767-791-1112

Fax 767-791-1890

### City Planning Commission of Pinole CA

May 19, 2016

Dear Council Members

I have been retained by Doctor Scott Lee for the purpose of consulting for the building and licensing of his surgery center for Medicare and AAAHC. I currently have over 25 years in development of new OSHPD facilities as well as retrofits, upgrades and tenant improvements. I work closely with architects and contractors to ensure that all requirements are met for the purpose of compliance and assure licensing for the client / facility.

The size / square footage of the facility does not dictate the number of staff requirement. OSHPD is simply a building requirement and nothing else. The OSHPD requirement is simply an across the board standard requirement for all freestanding clinical settings and does not take into account the type of specialty. Therefore, in some cases like this one, the OSHPD building requirement is what would be considered an over build due in large part because the regulation is a generalization of requirements.

The attached document outlines the OSHPD 3 requirements for an ASC. Much of the space is not what is used on a practical daily basis. E.g. There is a requirement for separate bathroom facilities with lockers for each of the following male employees, female employees, patients. There is a separate anesthesia work room which no anesthesiologist in practice uses. There is a separate supervisor office, which is redundant with the front desk. There is a requirement for space and public access to a phone, when patients typically use a cell phone. The substerile, dirty, sterile work areas are also required and increase the square footage with significant redundancy. The code specifies many spaces and areas required, which increase the minimum square footage of such a facility, when that space may not practically be used.

Further, Ophthalmology is a specialty where the demographic of most patients is age 70 and visually impaired. If the patient is having a surgical service regardless of age they are not allowed to drive themselves.





## Healthcare Business Solutions

business solutions for outpatient centers

409 Mendocino Ave, Suite D

Santa Rosa, CA 95401

Office Tel: 707 535-3159

Fax: 707 535-3897

Family members of patients are asked to drop off the patient and leave then return when they receive a call from the facility to pick up the patient. This is due to the length of time the patient will be in the facility.

In the last year I developed 4 new construction facilities, and 7 remodels/tenant improvements some larger than this current project and in regards to parking we have only had to provide parking for staff. The last ASC had greater square footage than this project and had only 5 parking spaces.

My calculations based on Medicare requirements for staffing are such that this facility will never have 31 staff members or patients at the same time. Even if both the clinic and surgery center were functioning simultaneously at maximum capacity, from a functional standpoint, my experience and calculations estimate this number to be around 19 staff and patients at any one time. This is based on staffing of 2 RNs, 1 surgical technician, 1 administrative staff member, 1 anesthesiologist for a surgeon doing routine ophthalmologic surgery, a 15 minute turnover.

Minimum staffing requirement is regulated by Medicare, the Board of Nursing and California Department of Public Health (CDPH) and upheld by the accreditation agencies that contract with them for the purpose of enforcement of the regulations for all physician owned facilities on behalf of Medicare and CDPH.

In regards to the code for minimum staffing requirements set forth by Medicare (416.42(b)(2), and CDPH (Q-0063), the requirement simply states;

“At a minimum, all settings in which sedation or anesthesia is administered should have the following minimum staff while patients are present within the facility 2 licensed ACLS RN’s, 1 physician present or immediately available. The remainder of staff required is to be within the standard of care for the specialty.”

Respectfully,

Victoria Haas, MHA, MBA

Healthcare Business Solutions



# GATEWAY MEDICAL CENTER PINOLE VALLEY RD. AT HENRY AVE., PINOLE, CA 94564

## DESIGN REVIEW PLANNING SUBMITTAL (DR 15-10 / CUP 15-02)

<b>PROJECT INFORMATION</b>	
OWNER:	AGAPE, LLC
APN:	401-021-033
ZONING DISTRICT:	PINOLE VALLEY ROAD SPECIFIC PLAN OFFICE PROFESSIONAL MIXED USE (OPMU)
LOT SIZE:	28,090 S.F.
BUILDING S.F.:	9,182 S.F.
CONSTRUCTION TYPE:	V-B
AUTOMATIC SPRINKLERS:	YES
NUMBER OF STORIES:	2
REQUIRED PARKING:	1 PER 250 S.F. = 37 SPACES

<b>DRAWING INDEX</b>		
NO.	TITLE AND PHOTO	DATE
11	ABSTRACT	1 OF 1
12	SITE PLAN	LANDSCAPE
13	LANDSCAPE PLAN	LANDSCAPE
14	FIRST FLOOR PLAN	PRELIMINARY LANDSCAPE PLAN
15	SECOND FLOOR PLAN	PRELIMINARY
16	EXTERIOR ELEVATIONS	1 OF 1
17	EXTERIOR ELEVATIONS	SITE & BUILDING LIGHTING CALCULATIONS
18	PHOTO SIMULATIONS	
19	MOONED PERSPECTIVES	

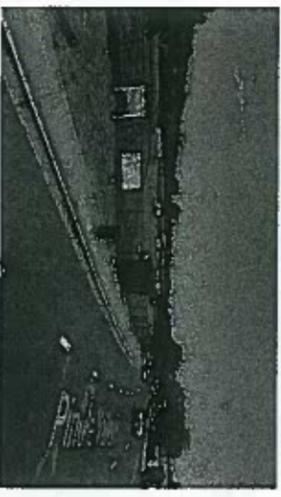
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PROJECT LOCATION



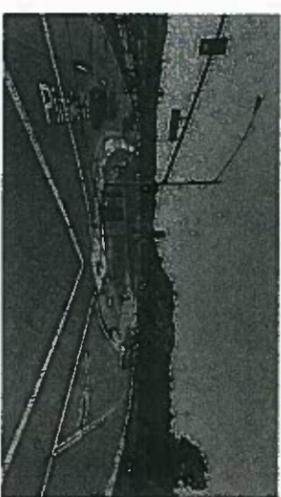
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(PINOLE VALLEY RD. AND HENRY AVE.)



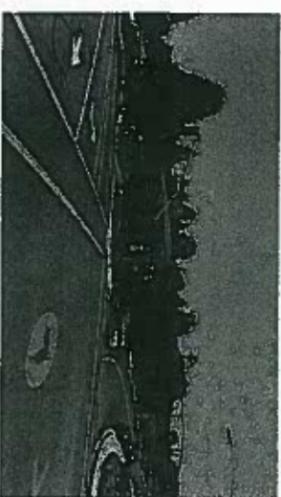
3 COLLINS ELEMENTARY  
(PINOLE VALLEY RD. AND HENRY AVE.)



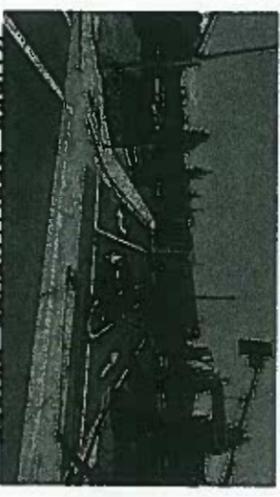
5 SINGLE-FAMILY RESIDENTIAL  
(HENRY AVE.)



2 VACANT LOT  
(PINOLE VALLEY RD. AND HENRY AVE.)



4 JULIE'S NAILS  
(PINOLE VALLEY RD. AND HENRY AVE.)



6 KAISER PERMANENTE PINOLE  
MEDICAL OFFICES  
(PINOLE VALLEY RD. AND HENRY AVE.)

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CITY OF PINOLE  
DEPARTMENT SERVICES DEPT

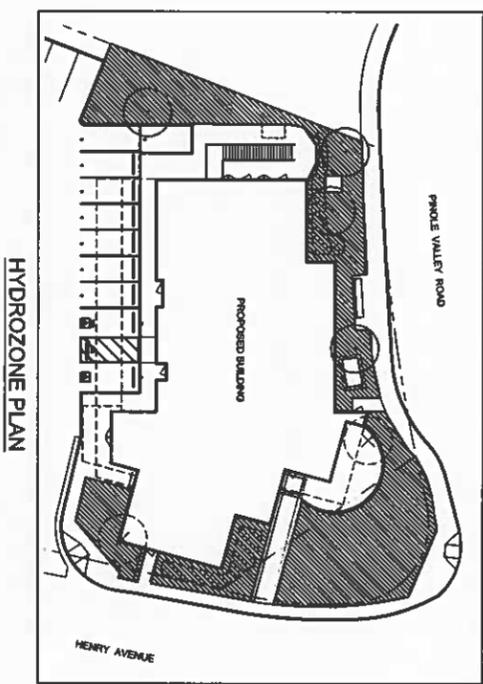
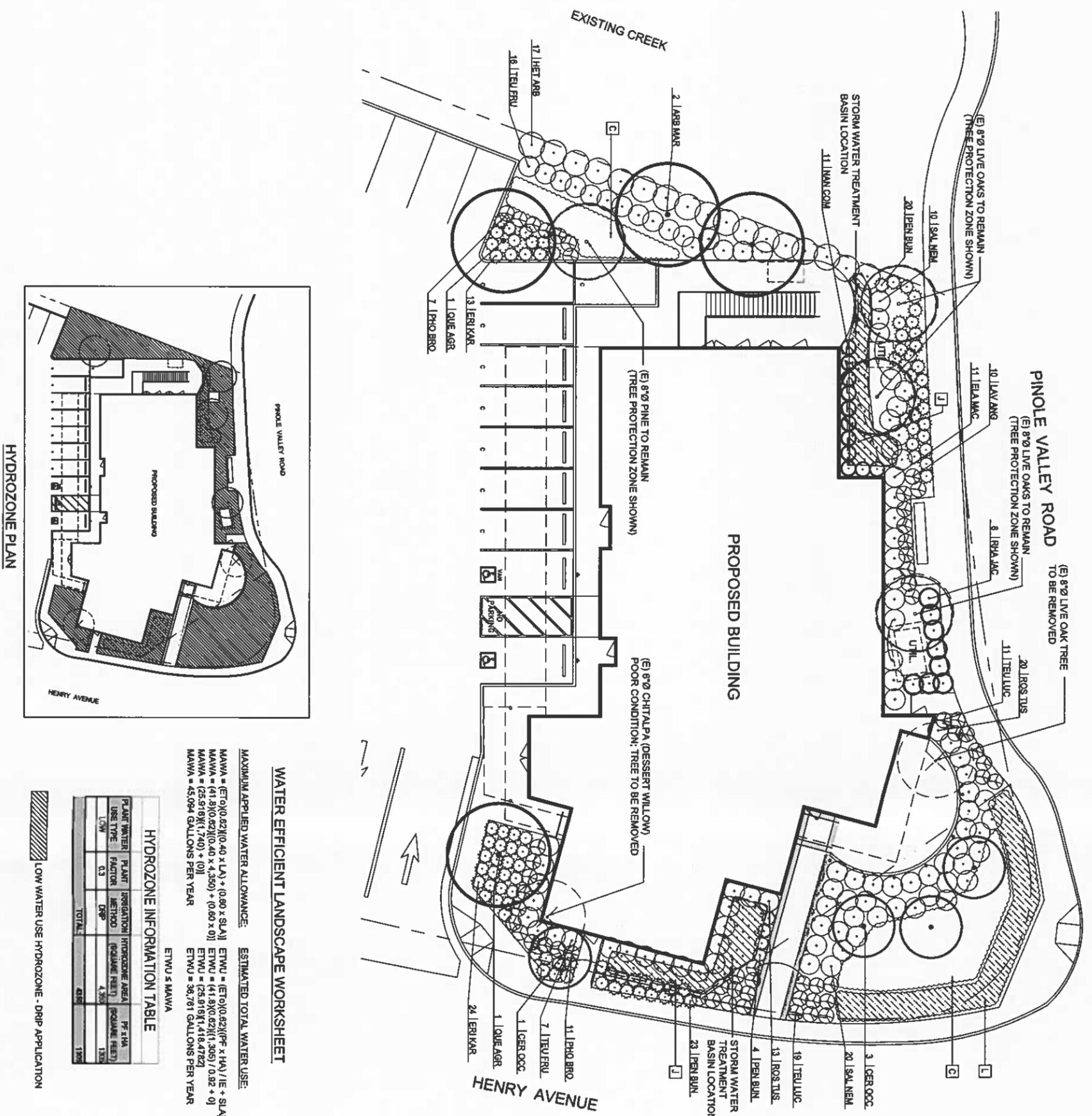


Questient Horton Architects  
1478 E. Shannon Ave, Suite 204  
Cupertino, California 95014  
408.271.2500 Fax 408.271.2501

GATEWAY MEDICAL CENTER  
PINOLE VALLEY RD. AT HENRY AVE., PINOLE, CA 94564  
AGAPE, LLC - 1214 McDONALD DR., PINOLE, CA 94564

TITLE AND PHOTO T 1  
11/24/16





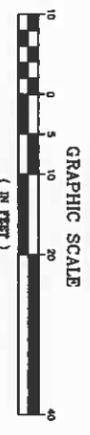
**WATER EFFICIENT LANDSCAPE WORKSHEET**

**MAXIMUM APPLIED WATER ALLOWANCE: ESTIMATED TOTAL WATER USE:**  
 MAWA = (ET<sub>0</sub>)(0.62)(0.40 x LA) + (0.80 x SLA) ETW<sub>0</sub> = (ET<sub>0</sub>)(0.62)(PF x HA) / IE + SLA  
 MAWA = (41.8)(0.62)(0.40 x 4,350) + (0.80 x 0) ETW<sub>0</sub> = (41.8)(0.62)(1,505) / 0.92 + 0  
 MAWA = (23,519)(0.740) + (0) ETW<sub>0</sub> = (23,519)(1,418.4782)  
 MAWA = 45,094 GALLONS PER YEAR ETW<sub>0</sub> = 36,781 GALLONS PER YEAR

**HYDROZONE INFORMATION TABLE**

PLANT WATER USE TYPE	PLANT FACTOR	IRRIGATION METHOD	HYDROZONE AREA (SQUARE FEET)	PF x HA	SQUARE FEET
LOW	0.3	DSP	4,350		1,305
<b>TOTAL</b>			<b>4,350</b>		<b>1,305</b>

LOW WATER USE HYDROZONE - DRIP APPLICATION



**PLANT MATERIALS LIST**

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	WATER USE
ABR	ABRUTUS MARIANA (STANDARD)	ABRUTUS STANDARD	24" BOX	LOW
OC	CERIS OCCIDENTALIS (STANDARD)	WESTERN REDBUD	24" BOX	LOW
QO	QUERCUS AGRIFFOLIA	CALIFORNIA LIVE OAK	24" BOX	LOW
<b>SHRUBS:</b>				
ELM	ELAENAGNUS PUN. MACULATA	GOLDEN ELAENAGNUS	5 GAL	LOW
HET	HETEROMELES ARBUSTIFOLIA	TOYON	5 GAL	LOW
NAN	NANDINA DOM. COMPACTA	COMPACT NANDINA	5 GAL	LOW
RHA	RYAHIOL-EPS JACK EVANS	INDIA HAWTHORN	5 GAL	LOW
ROS	ROSA MAGIC CARPET RED	RED CARPET ROSE	2 GAL	MED
ROS	ROSMARINUS TUSCAN BLUE	TUSCAN ROSEMARY	5 GAL	LOW
TEU	TEUCRUM FRUTICOSANS	BLUE GERMANDER	5 GAL	LOW
TEU	TEUCRUM LUCIDREYS	DWARF GERMANDER	5 GAL	LOW
<b>PERENNIALS AND GRASSES:</b>				
ERI	ERIGERON KARVINS. MOERHEIMII	SANTA BARBARA DANBY	1 GAL	LOW
LAV	LAVANDULA ANGSTIFOLIA	ENGLISH LAVENDER	1 GAL	LOW
PEN	PENISTEMUM RED BLUNNY TAIL S'	DWARF FOUNTAIN GRASS	1 GAL	LOW
PHO	PHORBIUM BRONZE BABY	DWARF FLAX	1 GAL	LOW
SAL	SALVIA NEMOROSA	PURPLE SAGE	5 GAL	LOW
<b>GROUND COVER:</b>				
COR	CORPUSMA KIRKII	CREeping CORPUSMA	1 GA CAN @ 36" LOW	LOW
JUN	JUNCA PATENS	GRAY RUSH	4 POTS @ 12" O.C.	LOW
LANT	LANTANA MONTEVIDENSIS	LANTANA	1 GA CAN @ 30" O.C.	LOW

**GENERAL NOTES:**

- ALL PLANTING SHALL BE WATERED BY A FULLY AUTOMATIC, WATER-CONSERVING IRRIGATION SYSTEM WITH WEATHER-BASE CONTROL.
- ALL PLANTING AREAS SHALL RECEIVE A 3" LAYER OF FIBRARK MULCH DRESSING, WITH EXCEPTION OF STORMWATER BASIN AREAS.
- THE EXISTING TREES TO REMAIN, ALL CALIFORNIA LIVE OAKS, ARE IN HEALTHY CONDITION.
- THE PROTECTION MEASURES ARE LISTED ON THIS SHEET.

**EXISTING TREE PRESERVATION GUIDELINES:**

- No construction activity shall be allowed within 'tree protection zone' (canopy drip-line) as identified on plans.
- This includes: grading, stockpiling, or placement of utilities. Trenching may take place under the direction of a Certified Arborist.
- No materials shall be stored or disposed of within tree protection zone or discarded up-slope of tree locations. No herbicides shall be applied within tree protection zones.
- No vehicles, equipment or supplies shall be driven or stored within tree protection zone.
- A 6" chain link fence (or other City approved fencing materials) shall define the tree protection zone at the drip-line of trees and shall be in place prior to beginning of any construction or demolition/clear and grub operations. City shall be notified after placement of fencing and shall inspect location prior to construction. Fencing shall remain in place until all construction on site is completed. Hay bales may be required for trunk protection at critical trees in addition to fencing.
- Overhanging limbs are to be protected by 2x4's or other approved material tied to the tree under direction of Certified Arborist.
- Any tree pruning either for the health of the tree or due to construction shall be under the direction of a Certified Arborist and work shall be done by a licensed tree service contractor. Irrigation for the existing tree, if necessary, shall be performed under the direction of a Certified Arborist.
- All drain water shall keep natural flow on the site if possible.
- Tree roots which are excessively damaged due to inadequate protection or negligence by the Contractor are to be replaced with like size and species at contractor's expense.
- Any trenching within dripline is to be avoided. If that is not possible, follow these recommendations:
  - Hand trench within 12" of trunk of existing trees. Use light equipment from 12" out to drip-line of the existing tree.
  - Cut roots with a straight cut. Do not break off. Cut back to downward heading lateral root if possible. Cut using sharp tools. Cover with same depth of soil as previously was covering surface immediately.
  - If trench is not immediately closed, cover with wet burlap until trench is closed.
  - All work done within drip line to be under the supervision of a Certified Arborist.

**CORNERSTONE SURGICAL CNTR.**  
**PINOLE VALLEY RD./HENRY AVE.**  
 PINOLE, CALIFORNIA



**Thomas Baak & Associates, LLP.**  
 Landscape Architects  
 1620 North Main Street, Suite 4  
 Walnut Creek, CA 94596  
 P: 925.933.2583

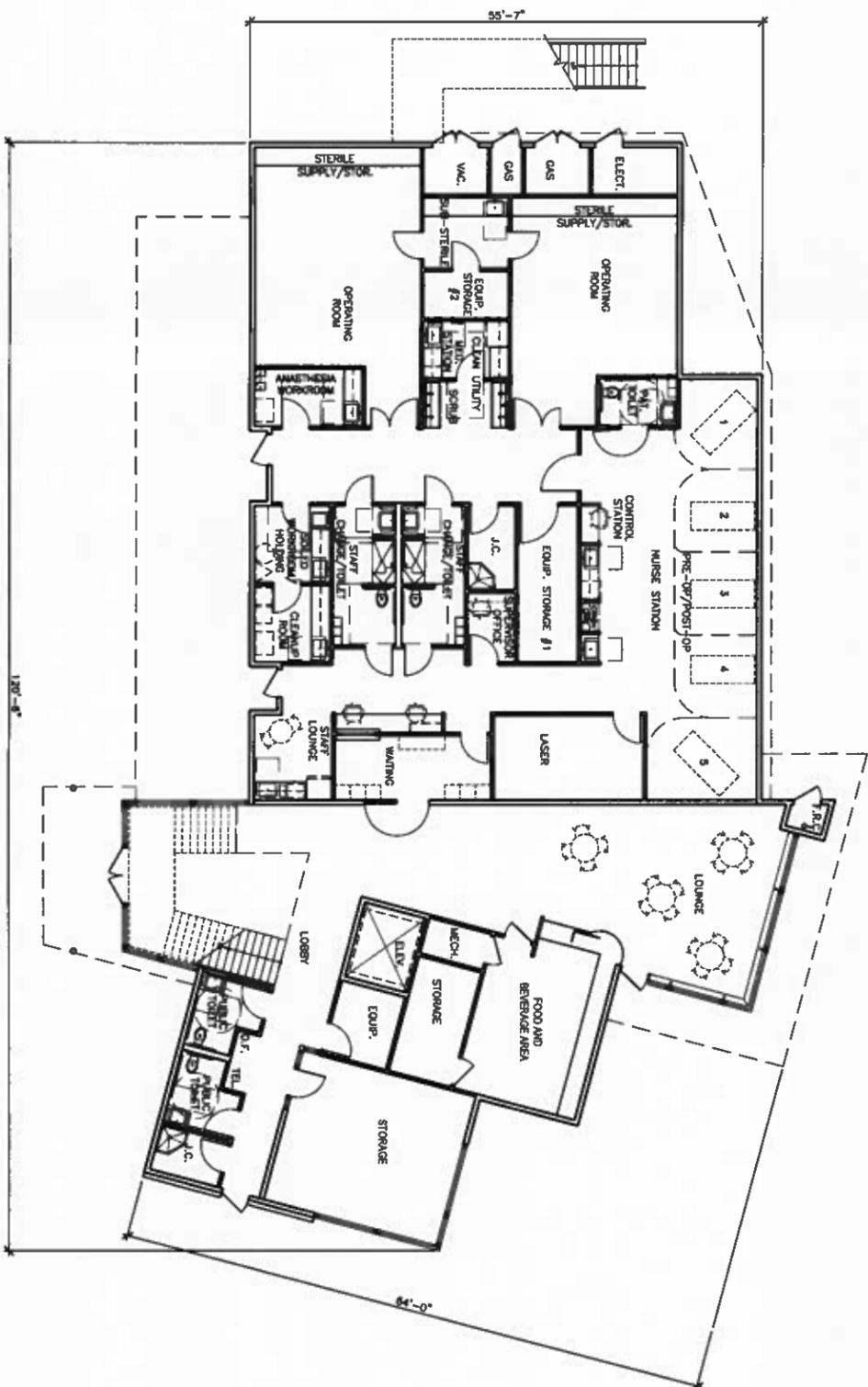
NO.	DATE	DESCRIPTION
1	10-28-15	PRELIMINARY LANDSCAPE PLAN

NO.	DATE	DESCRIPTION
1	10-28-15	PRELIMINARY LANDSCAPE PLAN

**PRELIMINARY LANDSCAPE PLAN**



Cuschliert Horton Architects  
 1475 S. Bascom Ave. Suite 204  
 Campbell, California 95008  
 408.371.2800 FX 408.371.2801

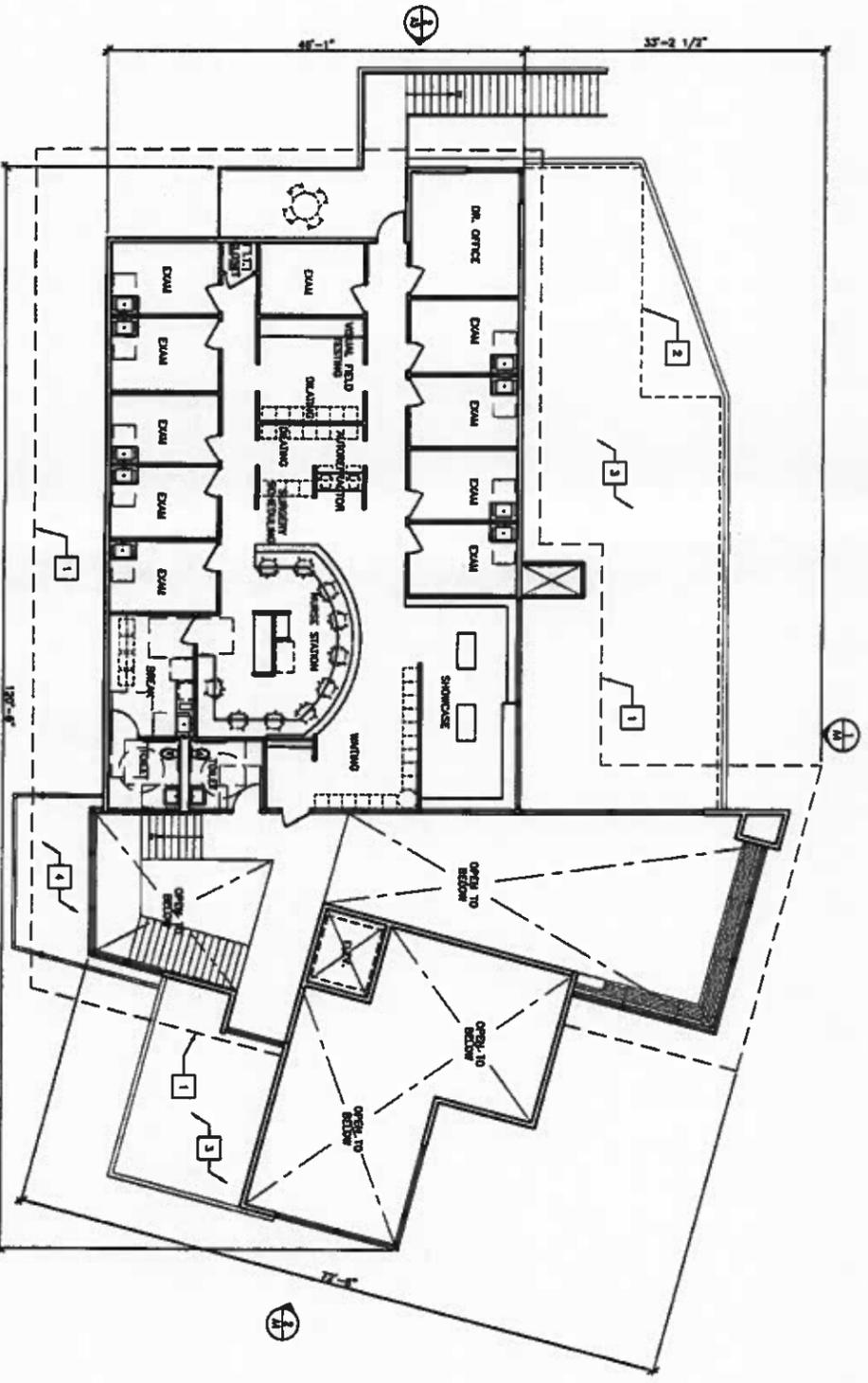


1 FIRST FLOOR PLAN



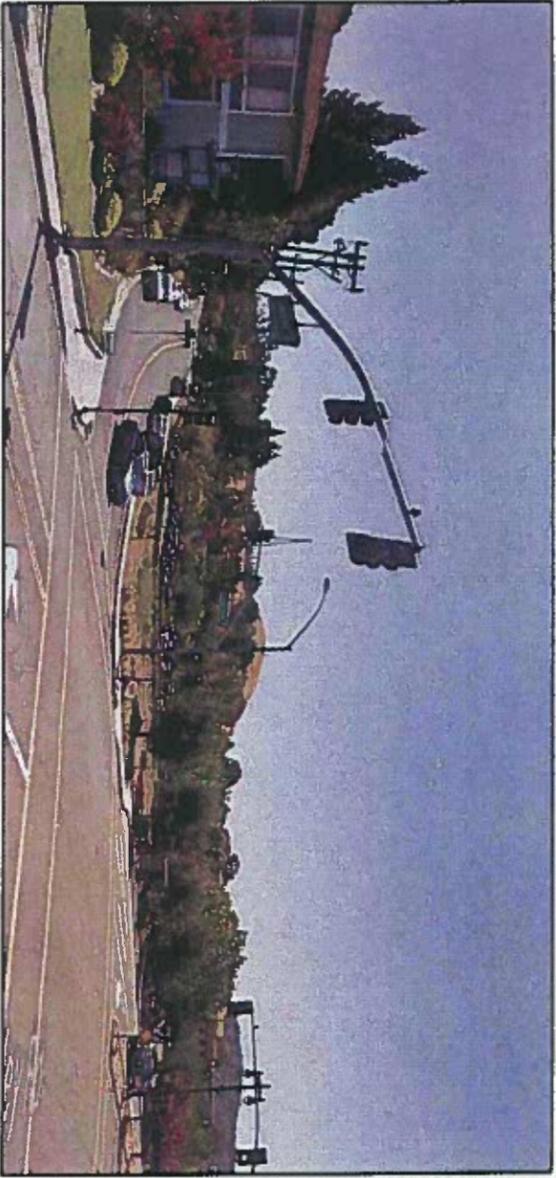
GATEWAY MEDICAL CENTER  
 PINOLE VALLEY RD. AT HENRY AVE., PINOLE, CA 94564  
 AGAPE, LLC - 1214 McDONALD DR., PINOLE, CA 94564

- SPECIFIC NOTES**
1. LINE OF ROOF ABOVE, TYP.
  2. LINE OF EXTERIOR WALL BELOW, TYP.
  3. ROOF BELOW, TYP.
  4. CHIMNEY ROOF BELOW, TYP.



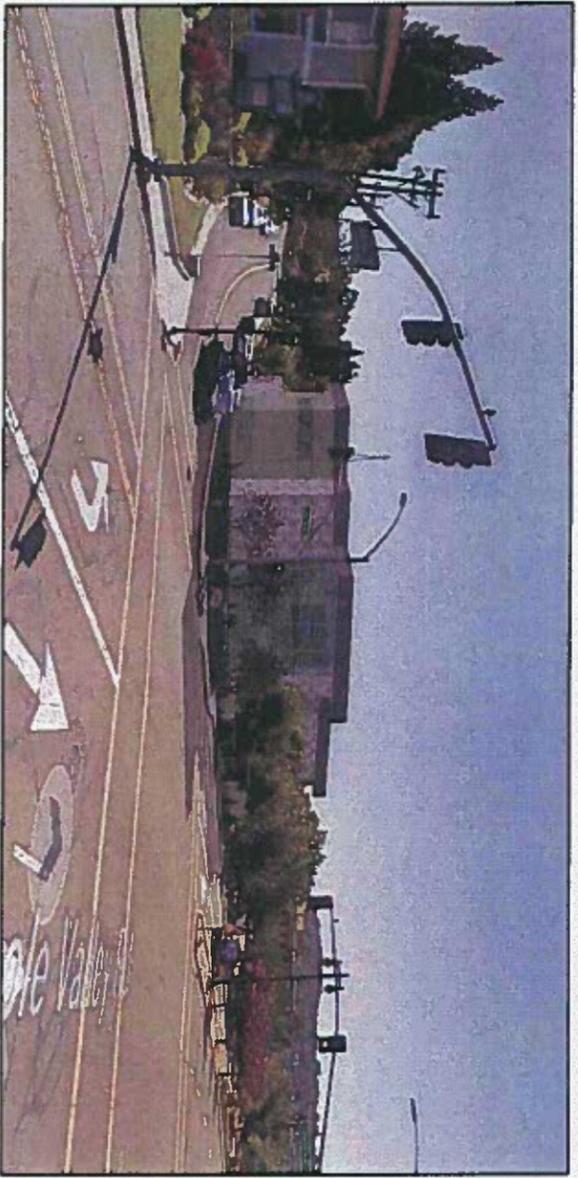
1 SECOND FLOOR PLAN





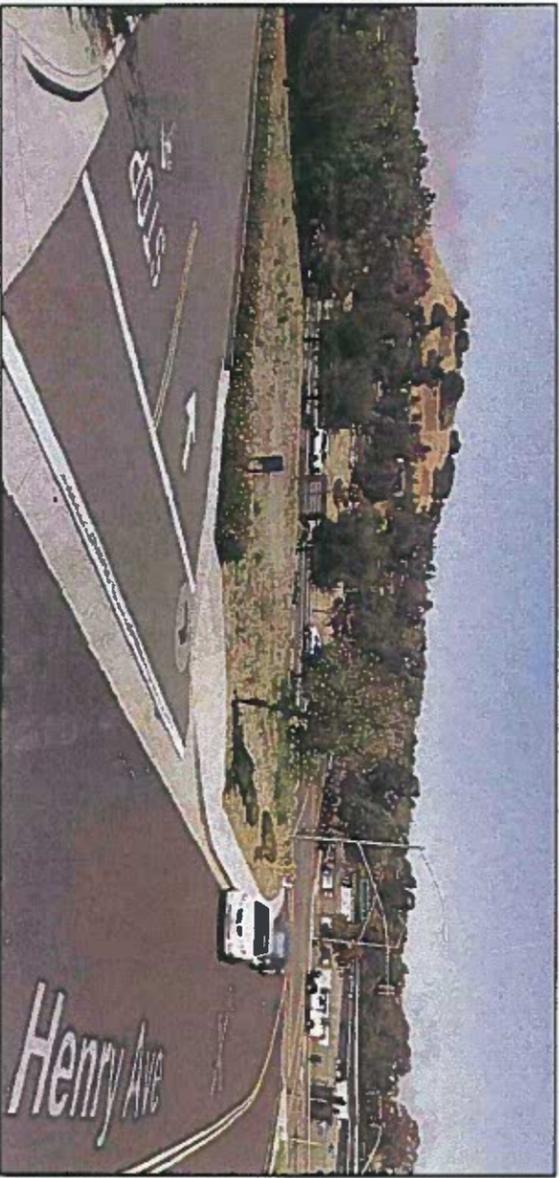
1 PINOLE VALLEY RD. SOUTHEAST VIEW - EXISTING

NIS



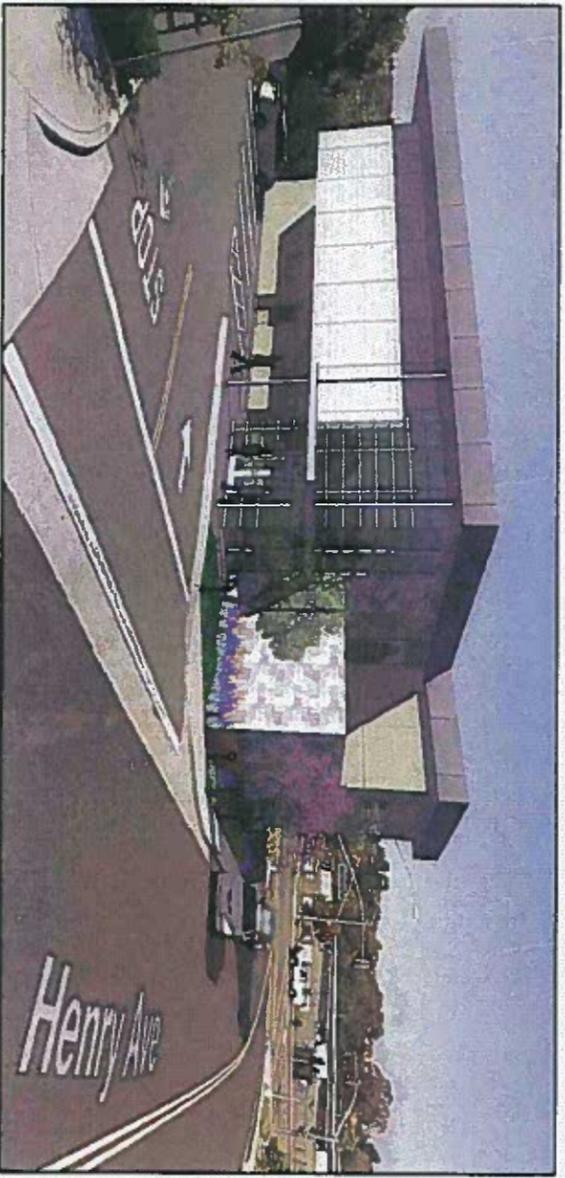
2 PINOLE VALLEY RD. SOUTHEAST VIEW - PROPOSED

NIS



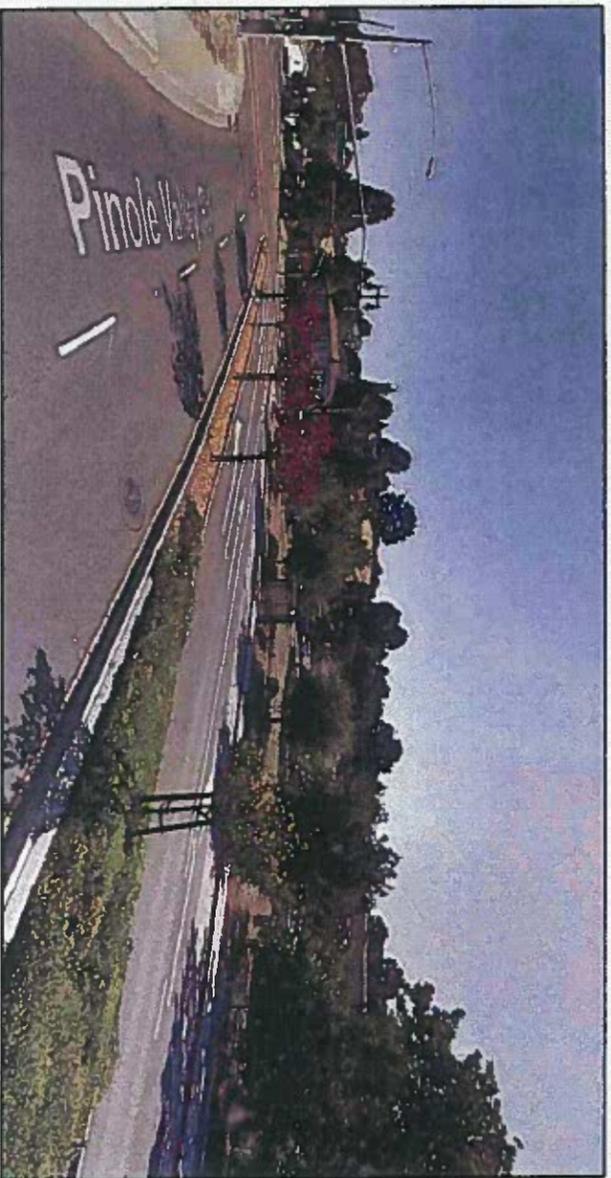
3 HENRY AVE. WEST VIEW - EXISTING

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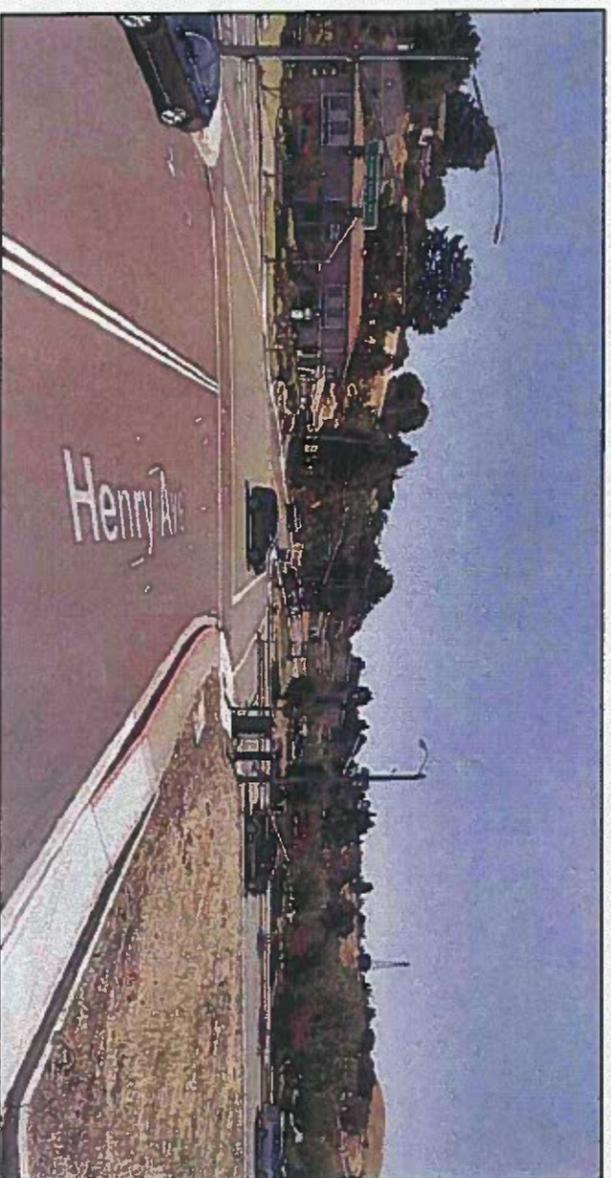
4 HENRY AVE. WEST VIEW - PROPOSED

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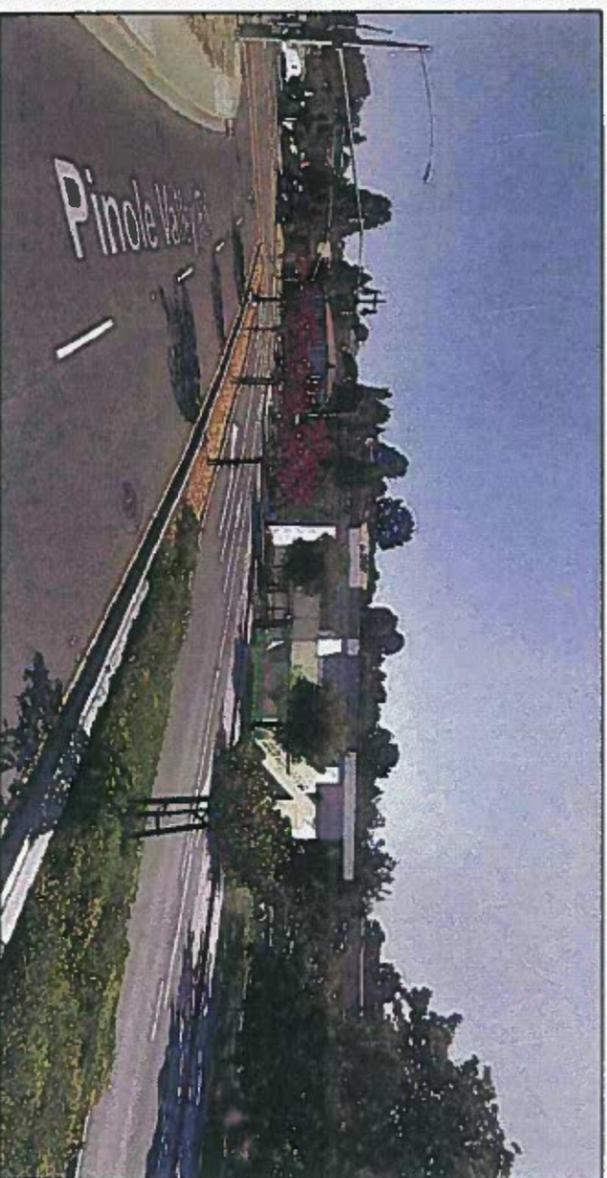
1 PINOLE VALLEY RD. NORTH VIEW -- EXISTING

NIS



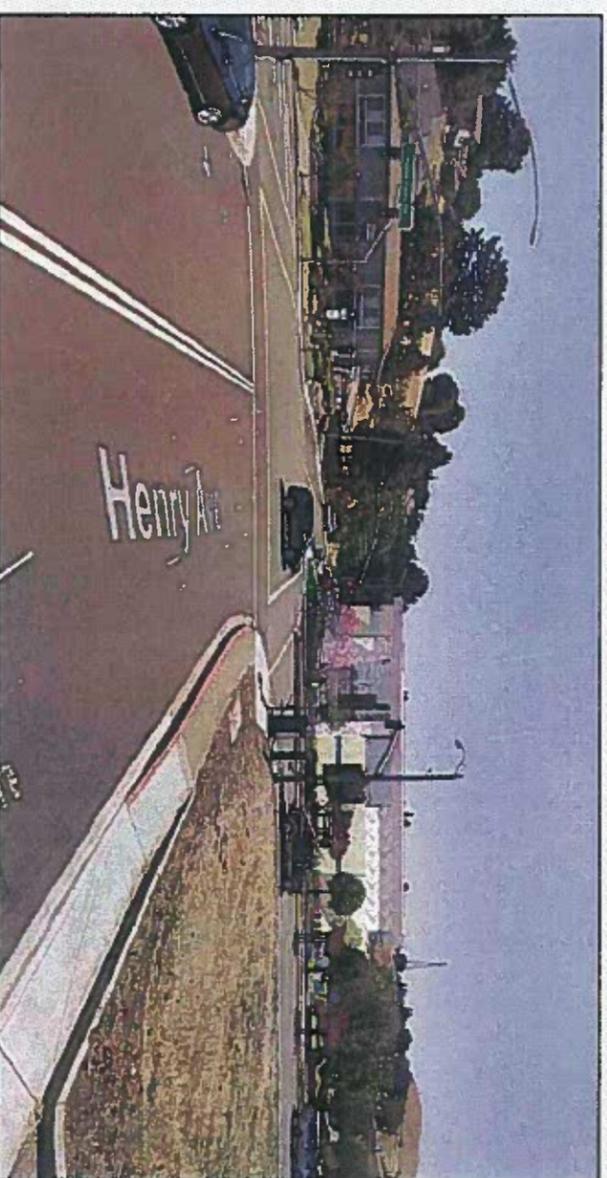
2 HENRY AVE. EAST VIEW -- EXISTING

NIS



3 PINOLE VALLEY RD. NORTH VIEW -- PROPOSED

NIS

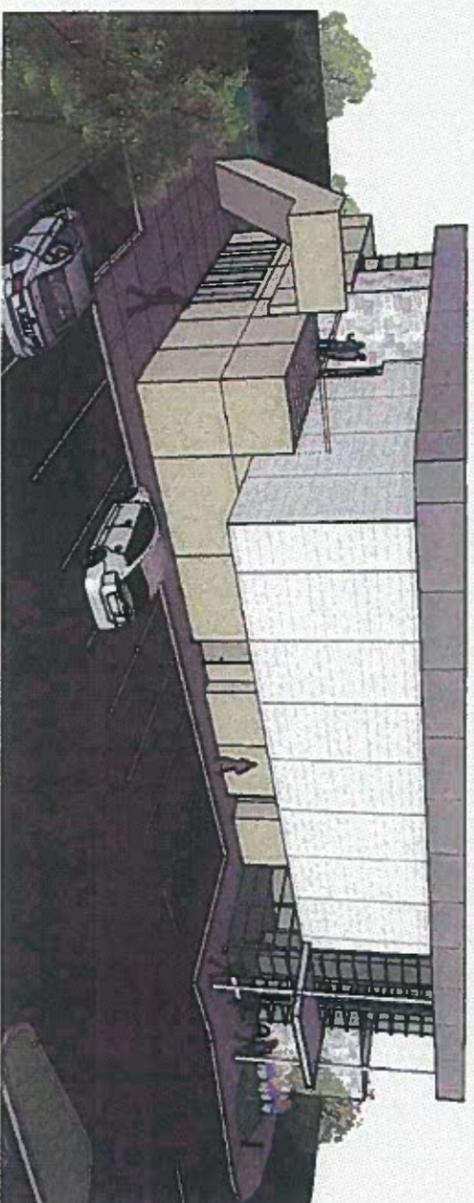


4 HENRY AVE. EAST VIEW -- PROPOSED

NIS



1 SOUTHEAST VIEW FROM PINOLE VALLEY RD./HENRY AVE. INTERSECTION



2 NORTHWEST VIEW FROM PARKING LOT



3 NORTHEAST VIEW FROM PINOLE VALLEY ROAD



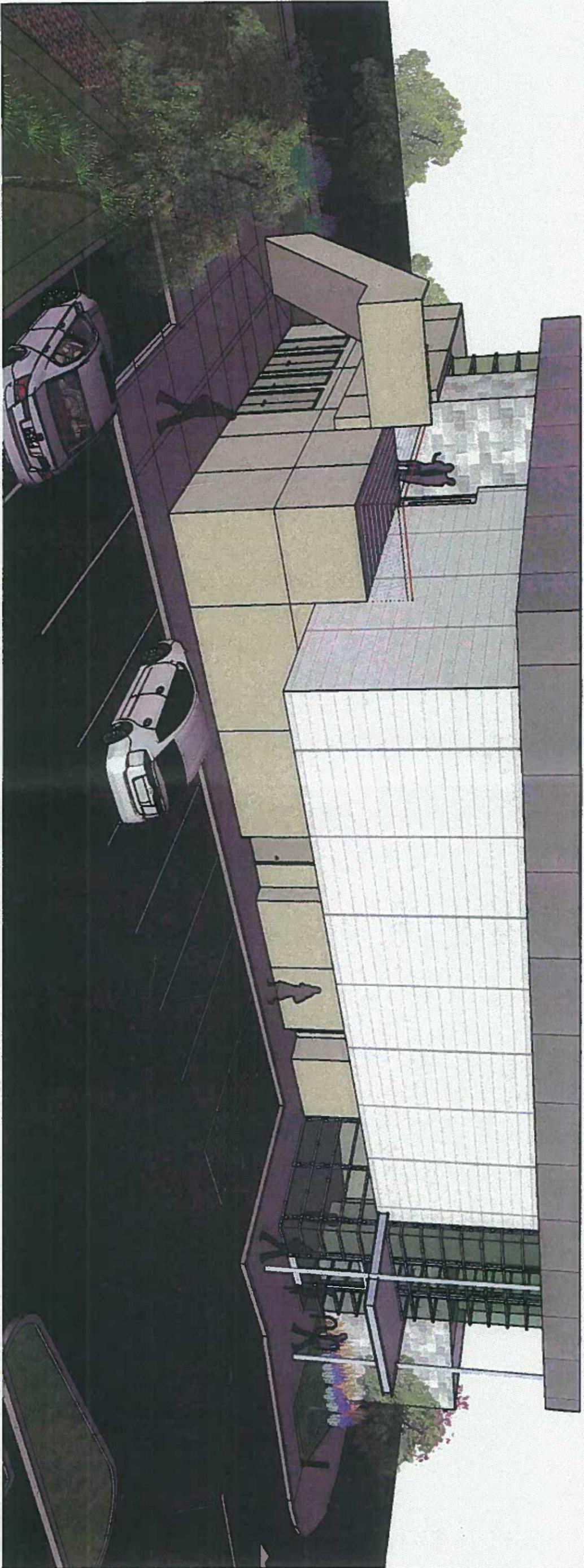
Caschieri Horton Architects  
4718 S. Bascom Ave., Suite 204  
Campbell, California 95008  
408.271.1200 Fax 408.271.8291

GATEWAY MEDICAL CENTER  
PINOLE VALLEY RD. AT HENRY AVE., PINOLE, CA 94564  
AGAPE, LLC - 1214 McDONALD DR., PINOLE, CA 94564

RENDERED PERSPECTIVES A7  
11/20/13

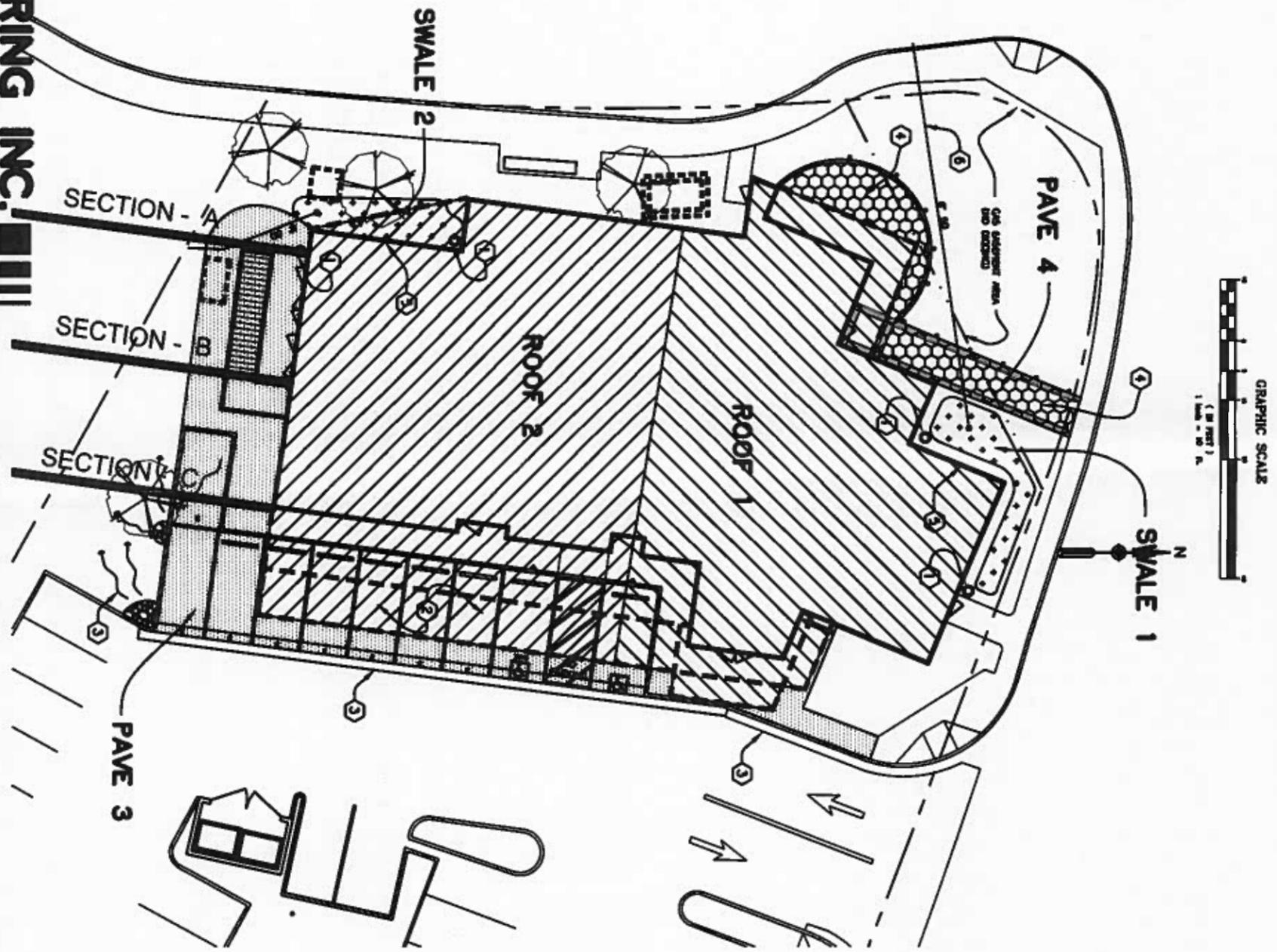








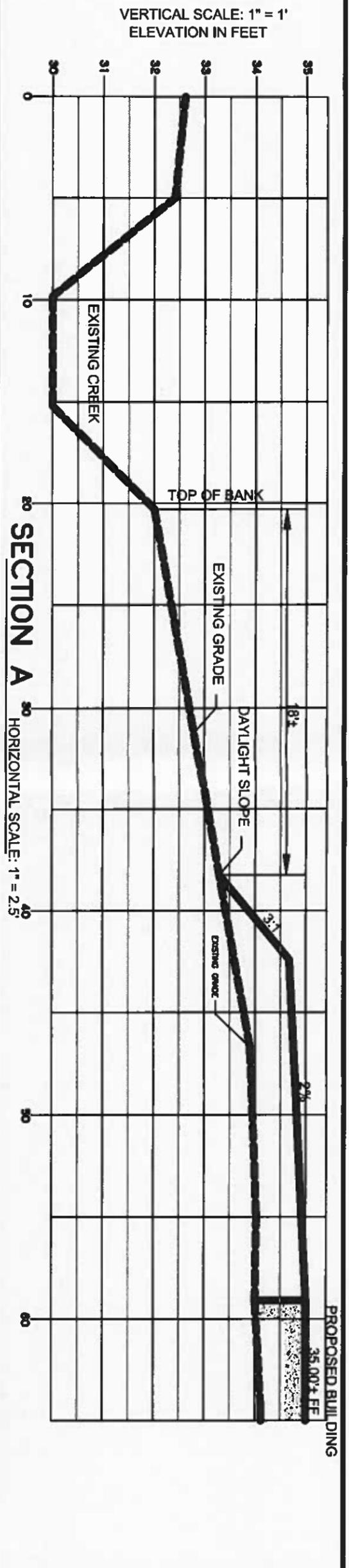
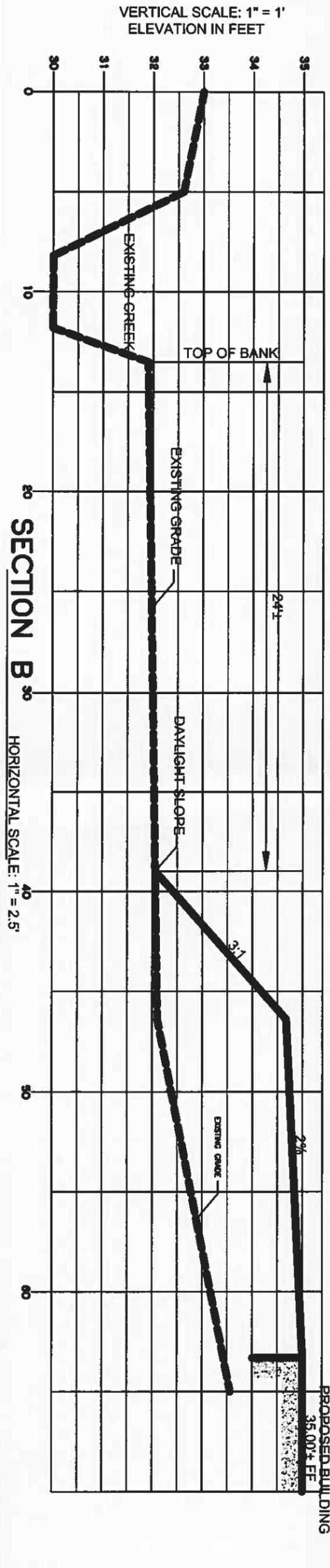
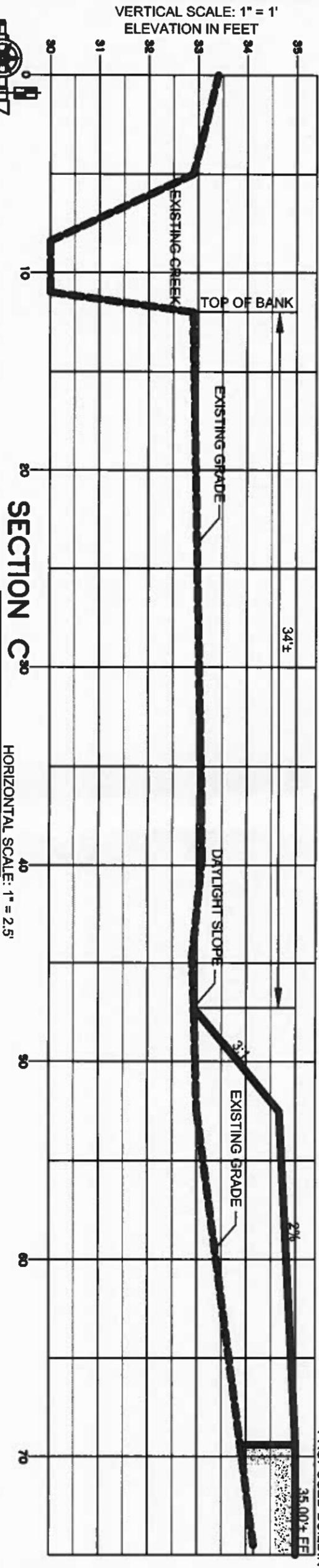
**CUNHA ENGINEERING INC.**



PRELIMINARY SECTION INDEX GATEWAY MEDICAL CENTER PINOLE VALLEY ROAD AT HENRY AVE PINOLE CALIFORNIA	<b>CUNHA ENGINEERING INC.</b> 701 BELMONT WAY, STE. A PINOLE, CALIFORNIA 94564 (510) 741-8290	DESIGNED BY:					
		DRAWN BY:					
		CHECKED BY:					
		SCALE:					
		APPROVED BY:					
		SHEET NO.:					
		DATE:					



**CUNHA ENGINEERING INC. ■■■■**



**PRELIMINARY CROSS-SECTIONS  
PHOLE SURGERY CENTER**

**CUNHA ENGINEERING INC.**  
701 BELMONT WAY, STE. A  
PHOLE, CALIFORNIA 94964  
(810) 741-8290

DESIGNED BY: [ ]  
DRAWN BY: [ ]  
CHECKED BY: [ ]  
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OF 1 SHEETS  
DATE: [ ]

PHOLE

CALIFORNIA

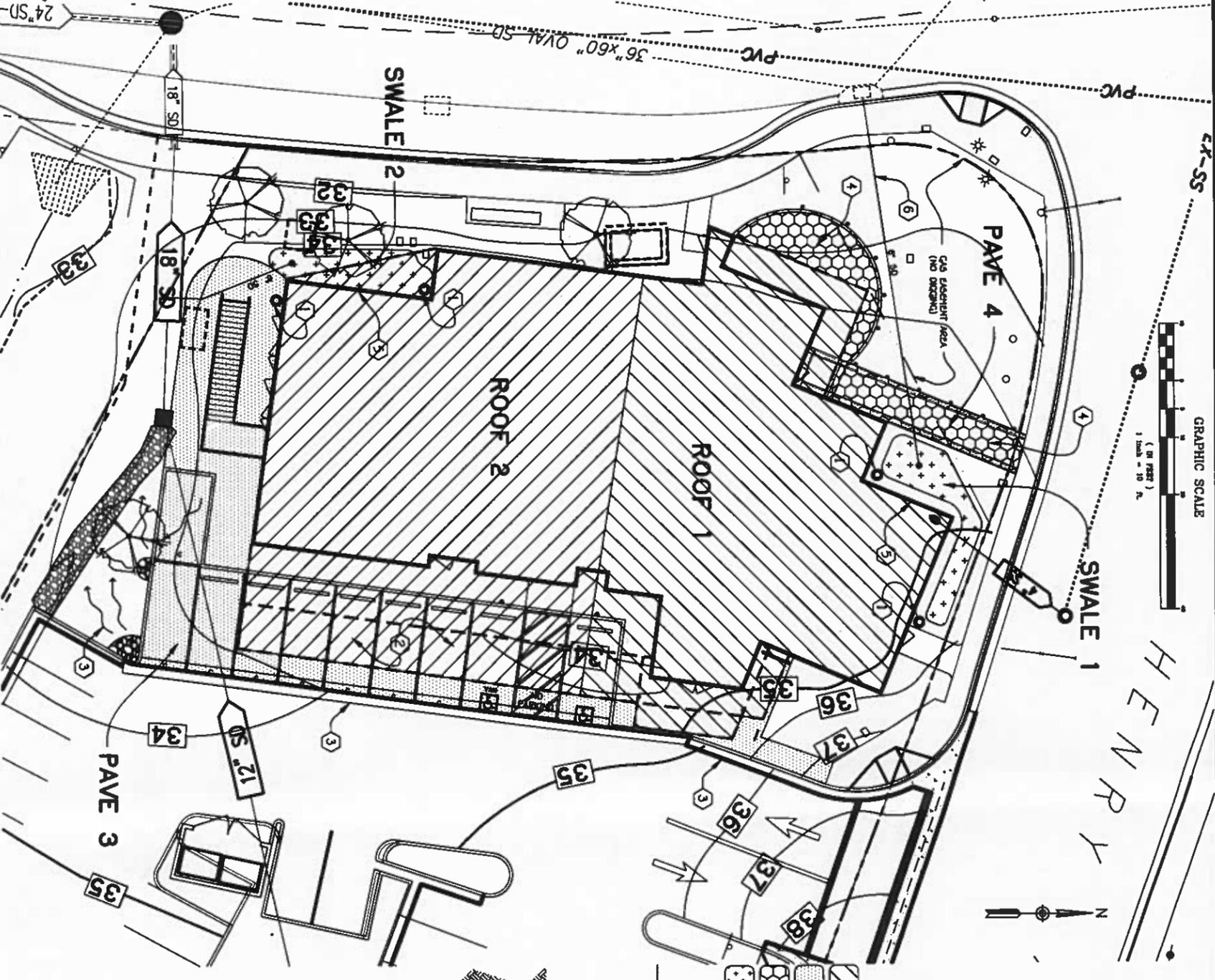
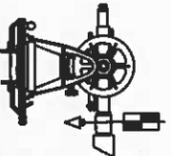
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CUNHA ENGINEERING INC. ■■■



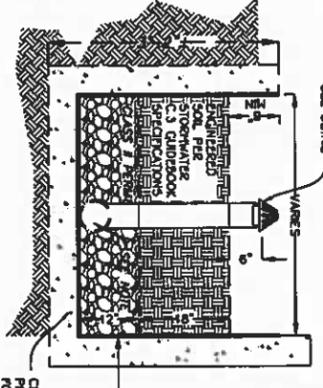
HENRY

- NOTES:
- 1 ALL ROOF LOADS SHALL DRAIN TO APPROPRIATE TREATMENT AREA.
  - 2 NINE NEW PARKING SPACES ON THE EAST SIDE OF THE BUILDING WILL BE COVERED BY ROOF. NO TREATMENT REQUIRED.
  - 3 INTERCEPTOR SWALE AT EDGE OF EXISTING PARKING TO COLLECT AND CONVEY RUN-OFF FROM EXISTING PARKING LOT, AND THEN SHEET FLOW THROUGH LANDSCAPING TO EXISTING SWALE.
  - 4 TREATMENT AREAS ARE SIZED TO TREAT ROOF RUN-OFF. PATIOS AND WALLS SHALL FLOW TO LANDSCAPE AREAS FOR TREATMENT.
  - 5 TREATMENT AREAS NEXT TO FOUNDATIONS WILL REQUIRE SPECIAL CONSIDERATION BY GEOTECHNICAL AND STRUCTURAL ENGINEERS.
  - 6 CONFINED WITH GAS LINE REPRESENTATIVES BEFORE DESIGN AND CONSTRUCTION OF STORAGE DRAIN WITHIN GAS LINE DASHENT.

CLEANWATER CALCULATIONS

ORGANIC MANAGEMENT AREA DESCRIPTION	DMA AREA (SF)	TREATMENT IMP AREA REQUIRED	IMP AREA PROVIDED
ROOF 1	3,692 SF	146 SF	182 SF
ROOF 2	4,389 SF	173 SF	173 SF
IMPERVIOUS AREA CALCULATION			
ROOF 1	3,692 SF		
ROOF 2	4,389 SF		
PAVE 3	1,665 SF		
TOTAL	9,686 SF		
IMPERVIOUS PAVEMENT CALCULATION			
PAVE 4	482 SF		

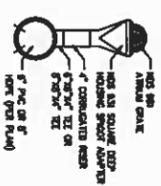
**TOTAL NEW AND REPLACED IMPERVIOUS AREA = 9,686 SF**



FLOW-THROUGH PLANTER  
NOT TO SCALE

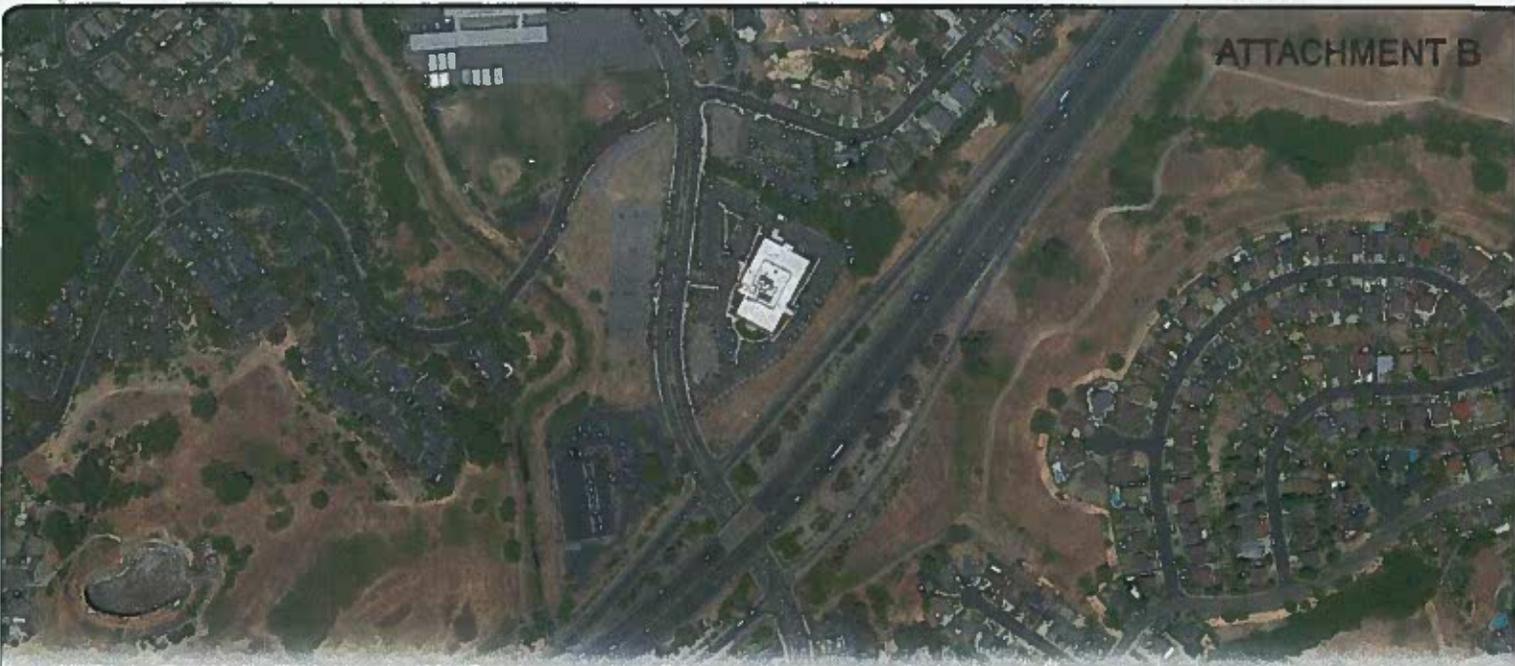
NDS INLET  
NOT TO SCALE

PARKING LOT GRADING  
NOT TO SCALE



NOTE: PERVIOUS PAVEMENT TO BE AT BOTTOM OF CLASS 1 PAVEMENT WHEN OPTIONAL CONCRETE BOTTOM IS INSTALLED. PERVIOUS PAVEMENT TO BE AT TOP OF CLASS 1 PAVEMENT WHEN THE PAVEMENT IS INSTALLED WITHOUT BOTTOM.

SHEET NUMBER OF 1 SHEETS DRAWING NO. 215145	<b>PRELIMINARY CLEANWATER CONCEPT</b> <b>GATEWAY MEDICAL CENTER</b> <b>PINOLE VALLEY ROAD AT HENRY AVE</b> PINOLE CALIFORNIA	<b>CUNHA ENGINEERING INC.</b> 701 BELMONT WAY, STE. A PINOLE, CALIFORNIA 94564 (510) 741-8290	DESIGNED BY: VHC	APPROVED BY: _____ R.C.E. No. _____ DATE _____	DRAWN BY: CAB	SCALE: 1"=10'	CHECKED BY: VHC	BY DATE DESCRIPTION	APPR. BY CITY DATE BY
			BY DATE DESCRIPTION		APPR. BY CITY DATE BY				



*Trip Generation and Parking Analysis*

**GATEWAY MEDICAL CENTER**

City of Pinole

Prepared by:

Abrams Associates

1875 Olympic Boulevard, Suite 210

Walnut Creek, CA 94596

# Gateway Medical Center Project

## *City of Pinole*

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### TRANSPORTATION AND PARKING

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#### 1) EXECUTIVE SUMMARY

This study provides information on the project's trip generation and parking demand and also presents the results of parking occupancy surveys conducted in the project area. This study also describes the regulatory setting; the criterion used for determining the significance of environmental impacts; and summarizes potential environmental impacts and appropriate mitigation measures. This study has been conducted in accordance with the requirements and methodologies set forth by the City of Pinole, the Contra Costa Transportation Authority (CCTA), Caltrans, and the applicable provisions of CEQA. A review of parking conditions in the area indicates the project wouldn't be expected to cause any significant parking problems. Although there may be days when the project's parking demand exceeds the available off-street parking the surveys indicated there is sufficient on-street parking in the area to accommodate any additional parked vehicles generated by the proposed project.

#### 2) PROJECT DESCRIPTION

As mentioned above, the proposed project is a commercial development proposed to include 9,182 square feet of medical office space. The project is located on the east side of Pinole Valley Road just south of Henry Avenue. All access to the site will be from one unsignalized driveway on Henry Avenue, which also serves as the entrance to an existing employee parking lot for Kaiser's Pinole Medical Offices.

### 3) REGULATORY CONTEXT

Existing policies, laws and regulations that apply to the proposed project are summarized below.

#### 3.1 State

The California Department of Transportation (Caltrans) has jurisdiction over State highways. Therefore, Caltrans controls all construction, modification, and maintenance of State highways, such as I-80 or SR 4. Any improvements to these roadways would require Caltrans' approval. The Guide for the Preparation of Traffic Impact Studies provides consistent guidance for Caltrans staff who review local development and land use change proposals. The Guide also informs local agencies about the information needed for Caltrans to analyze the traffic impacts to state highway facilities which include freeway segments, on- or off-ramps, and signalized intersections.

#### 3.2 Local

**Contra Costa Countywide Comprehensive Transportation Plan Update (2009)** - The transportation policies that are currently applicable within Contra Costa County are based on the Contra Costa County Comprehensive Transportation Plan. This document identifies standards and procedures for analyzing transportation impacts in the county and includes action plans for routes of regional significance such as the West County Action Plan covering the project area.

**City of Pinole General Plan** - The Transportation and Circulation Element included in the City of Pinole General Plan was prepared pursuant to Section 65302(b) of the California Government Code. The Transportation and Circulation Element addresses the location and extent of existing and planned transportation routes, terminals, and other local public utilities and facilities. The General Plan identifies roadway and transit goals and policies that have been adopted to ensure that the transportation system of the City will have adequate capacity to serve planned growth. These goals and policies are intended to provide a plan and implementation measures for an integrated, multi-modal transportation system that will safely and efficiently meet the transportation needs of all economic and social segments of the City.

#### 3.3 Significance Criteria

It is important to note that parking impacts are no longer considered a significant impact under CEQA. SB 743 specified that parking impacts for qualifying infill projects are not considered significant impacts on the environment under CEQA. The fact that an urban development project does not "self-park" is not in itself a CEQA impact, but any environmental impacts foreseeably resulting from a project's "on-site" parking deficit should be analyzed and mitigated. In other words, if a project would result in additional vehicles parked on-street then the project should be evaluated to determine if this could result in other ancillary environmental impacts:

According to CEQA guidelines, a project would have a significant impact if it would:

- Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including, but not limited to, intersections, streets, highways and freeways, pedestrian and bicycle paths and mass transit.

- Conflict with an applicable congestion management program, including, but not limited to, level-of-service standards, and travel demand measures, or other standards established by a county congestion management agency for designated roadways.
- Result in inadequate emergency vehicle access.
- Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities.
- Result in an internal circulation system that does not meet City standards.

Please note the City of Pinole has not adopted standards for on-street parking occupancy levels so having high parking occupancy levels is not necessarily a significant impact unless, for example, this condition results in a significant safety problem or impedes emergency vehicle access.

## 4) IMPACTS AND MITIGATION MEASURES

### 4.1 Project Trip Generation

The proposed project will consist of 9,182 square feet of medical office space. The trip generation calculations are shown in **Table 1**. They are based on rates for a Medical-Dental Office Building from the Institute of Transportation Engineer's (ITE) Trip Generation Manual, 9<sup>th</sup> Edition.

The total trip generation reflects all vehicle trips that would be counted at the project driveways, both inbound and outbound. Although there is the potential for transit and bicycle use at this particular site, no reduction has been applied to the project trip generation to be conservative. It should be noted that based on information provided by ITE on trip reductions for developments located adjacent to bicycle lanes and/or bus transit corridors the project could potentially qualify for a 5% reduction to the project trip generation.<sup>1</sup> As shown in **Table 1**, the project is forecast to generate approximately 20 net new vehicle trips on the surrounding roadway system during the AM peak hour and 31 trips during the PM peak hour.

Based on the potential for transit and bicycle use a 5% reduction has been applied to the project trip generation. This is based on information provided by ITE on trip reductions for developments located adjacent to bicycle lanes and/or bus transit corridors. These reductions only apply when direct, safe connections will be made between the project and nearby transit stops.

For purposes of determining the reasonable worst-case impacts the trips generated by this proposed development are estimated for the peak commute hours of 7:30 a.m. and 8:30 a.m. and 4:30 p.m. and 5:30 p.m., which represent the peak of "adjacent street traffic". This is the period when the project traffic would generally contribute to the greatest amount of congestion.

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<sup>1</sup> ITE Trip Generation Handbook, 2<sup>nd</sup> Edition, Institute of Transportation Engineers, Washington D.C., June 2004.

**TABLE 1  
TRIP GENERATION CALCULATIONS**

Land Use	ITE Code	Size	ADT	AM Peak Hour			PM Peak Hour		
				In	Out	Total	In	Out	Total
ITE Medical-Dental Office Rates	220		36.13	1.89	0.50	2.39	1.00	2.57	3.57
Unadjusted Project Trip Generation		9,182 sq. ft.	315	17	5	22	9	24	33
Adjustment for Proximity To Transit (5% reduction)			-16	-1	0	-1	0	-2	-2
Project Trip Generation		9,182 sq. ft.	299	15	5	20	9	22	31

**SOURCE:** Institute of Transportation Engineers Trip Generation Manual (9<sup>th</sup> Edition) and the Trip Generation Handbook (2<sup>nd</sup> Edition)

## 4.2 Internal Circulation and Access

No internal site circulation or access issues have been identified that would cause a traffic safety problem or any unusual traffic congestion or delay. The site distance at the proposed entrance was reviewed for potential safety problems with vehicles and/or pedestrians and based on Caltrans sight distance standards it was also found to meet the minimum requirements for a private driveway. However, with respect to landscaping it should be noted that all ground cover adjacent to the project driveway should be trimmed to be no higher than 2 feet and any trees should be limbed up to at least 6 feet. Based on a review of the parking geometrics of the existing and proposed parking areas it appears that all parking aisles and parking spaces will meet City standards (subject to final City approval). It should be also noted there have been no significant safety problems reported with the operation of the existing parking lot on the site.

## 4.3 Parking Impacts

This section discusses the City of Pinole's zoning and estimated parking demand for the project. The project is proposing to provide six spaces less than off-street parking required according to the City's Municipal Code. As per the City's Municipal Code the minimum off-street parking requirement equates to one space per every 250 square feet of gross floor area. This equates to a requirement of 37 spaces.

**Residential Parking Demand Based on ITE Parking Generation Rates** - To provide additional information on the project's parking demand **Table 2** provides a summary of the parking demand results using the average ITE parking generation rates. According to the project description the project would operate differently on different days of the week. Exam days would be Monday through Friday with an estimated 20 to 50 patients per day. Two days a week would also be surgical days (Tuesday and Thursday) where there would be an estimated 12 patients per day for surgical procedures. According to the applicant this is the maximum schedule assuming there are two medical doctors practicing on the site. The parking demand estimates provided have been presented for the Medical-Dental Office Building Category (ITE Land Use Code 720).

As shown in **Table 2**, the maximum parking demand generated by the project would be forecast to be approximately 29 parking spaces on surgical days based on the ITE data. The number of

employees can also sometimes be a good indicator of the potential parking demand, although it should be noted this is usually not codified as part of the approvals.

**Table 2**  
**Off-Street Parking Calculations Using Parking Demand Data**  
**from the Institute of Transportation Engineers**

ITE Code	Project	Data Source	Land Use Category	Size		Parking Ratio	Required Spaces
720	Medical Office Building	ITE Parking Demand Rates	Medical Office	9,182	sq. ft.	3.20	29

**Additional Discussion on Acceptable Parking Occupancy Levels** – It is important to note that one “*rule of thumb*” for parking design is that parking in an area (or parking lot) is theoretically perceived by the general public to be full when more than 90% of the spaces are full.<sup>2</sup> As a result, it is typically desirable to have a parking supply at least 10 percent larger than the demand so that motorists are not discouraged from using off-street parking during peak periods. This is why the average parking supply ratio for medical office buildings (at the 77 sites surveyed for the ITE rates) is normally 4.0 spaces per 1,000 square feet of gross floor area. In other words, the average parking supply of the buildings surveyed by ITE was actually identical to the City’s zoning requirements (1 space per 250 square feet). Another parking demand guideline to keep in mind is the 80% parking occupancy threshold. Although there is no solid evidence to support it, observations indicate that the occurrence of illegal parking tends to increase as parking occupancy exceeds 80% over a large area.<sup>2</sup>

**On-Street Parking** - There are 123 on-street parking spaces within the project study area, i.e., on blocks that are within 500 feet of the project. On a typical weekday afternoon, there are approximately 50 on street spaces available within 500 feet of the project site.

**On-Street Parking Surveys** - In order to evaluate the local parking situation, on-street parking occupancy surveys were conducted while schools were in session based on direction provided by the City of Pinole. This survey includes a detailed inventory of all on-street and off-street parking within 500 feet of the project. The study involved a block-by-block survey of the number and types of spaces, and the parking occupancy on three different weekday mornings and afternoons (see Tables 1 through 4 for survey dates and times). The results of the study are attached to this report. Please note the new parking spaces being created for Kaiser on the eastern end of their property near the Gateway East Parcel were not yet available at the time of our surveys.

The number of parking spaces on each block-face are shown in **Table 1**. **Figure 1** shows the study area and the particular streets and off-street parking lots that have been studied. As seen in **Table 1**, there are 123 on-street parking spaces located on blocks within 500 feet of the project. The studies found that during the afternoon, there are about 70 spaces (57%) that are occupied, and about 50 spaces available. **Tables 2, 3, and 4** attached to this report present the detailed survey results for each block and for each area of the Kaiser parking lots. **Figures 2, 3, and 4** present the parking occupancy levels for each of the streets in the study area during the peak period recorded on each of the three days surveyed.

<sup>2</sup> ITE Parking Generation, 4<sup>th</sup> Edition, Institute of Transportation Engineers, Washington D.C., 2010

**Off-street Parking Lots** - There are no City of Pinole off-street public parking areas within 500 feet of the project. However, it should be noted that there are private parking lots for Kaiser's Pinole Medical Offices. The parking occupancy levels in the existing Kaiser parking lots were also recorded during each of the surveys. As seen in Table 1, there are 342 off-street parking spaces in the Kaiser parking lots adjacent to the proposed project. The studies found that during the peak demand in the early afternoon there are typically about 250 spaces (73%) that are occupied, and about 90 spaces available. However, it should be noted that parking area #1 (the lot adjacent to the proposed project site) was found to generally be about 90% occupied during the mid-afternoon peak period of each survey.

**Shared Parking Considerations With Kaiser** – It is important to note that the proposed project would displace 22 parking spaces in the northern parking lot currently used by Kaiser employees (identified as Lot #1 in Figure 1). Please note that 21 of these spaces would become designated parking spaces for the proposed project and one space would be removed to create a new trash enclosure for the project. Kaiser has previously been authorized to utilize 22 of the parking spaces in this lot that they do not own and, as a result, this project would take over 22 parking spaces currently being used by Kaiser Employees. Since Lot #1 is typically over 90% occupied it is assumed that approximately 22 employee vehicles will need to be relocated to the other Kaiser parking lots closer to the main Kaiser building. Based on the parking surveys this would be expected to increase the occupancy levels in the lots surrounding the main Kaiser building from the existing maximum recorded occupancy levels of 73% to about 80% if about 22 employee vehicles are shifted this lot. Please note this could be partially off-set with the removal of containers and other equipment that is currently blocking about five spaces in the main lot.

**Summary of Findings on Parking** - Based on the parking surveys of the surrounding neighborhood and the parking requirements for medical dental office space (1.0 per 250 sq. ft.) it is estimated that the parking demand could exceed the supply being proposed (31 spaces) by approximately 6 vehicles. However, given the City's policies supporting alternative transportation and the project's close proximity to bus transit the City could consider making the findings that the proposed 31 space parking supply for the project is reasonable and appropriate.

The justification could be as follows:

- 1) The project will also provide additional publicly accessible bicycle racks.
- 2) There are numerous shopping, employment, and education centers within walking distance of the site (such as the nearby planned Starbucks and the Sprouts Market being constructed directly across the street).
- 3) There is extensive public transportation available in the project area provided by WestCAT, which provides connections to the El Cerrito Del Norte BART Station. The WestCAT routes that runs closest to the proposed project are routes 16, 19, and JPX. These routes all have stops on Pinole Valley Road adjacent to the project site.

Based on Section 17.48.040 of the Pinole Municipal Code the Planning Commission will only grant a conditional use permit for reduced parking if it finds that the project meets all of the conditional use permit criteria in Section 17.12.140 (Conditional Use Permits) and that three (3) or more of the circumstances listed below are true.

- a) *The use will be adequately served by the proposed parking due to the nature of the proposed operation; proximity to frequent transit service; transportation characteristics of persons residing, working, or visiting the site; or because the applicant has undertaken a travel demand management program that will reduce parking demand at the site.*

The applicant has proposed the following travel demand management program for the project:

- 1) All employees will have available vouchers that pay 100% of their public transportation expenses.
- 2) The site is located next to a bus stop which services bus lines 16, 19 and JPX.
- 3) An employee ride share program will be instituted allowing employees subsidizing transportation costs, share commuter lanes and reduce by ½ the necessary parking needed on site.
- 4) The applicant bikes to work and other individuals would be encouraged to do so with bike racks prominently featured in the front of the building.

This circumstance appears to be met with the presence of the adjacent bus stops and the proposal to provide transit vouchers for all employees. With financial incentives to use transit ITE data indicates the project parking demand could be reduced by as much as 16 percent. This could potentially offset the requested reduction in parking by reducing the overall project parking demand by about 6 spaces.

- b) *Parking demand generated by the project will not exceed the capacity of or have a detrimental impact on the supply of on-street parking in the surrounding area.*

This circumstance appears to be met since there is sufficient capacity on the surrounding street system to absorb at least an additional 6 vehicles without causing the parking occupancy levels to exceed 80% in the area.

- c) *The site plan is consistent with the objectives of the zoning district and incorporates features such as unobtrusive off-street parking placed below the ground level of the project with commercial uses above or enclosed parking on the ground floor.*

This will be determined by City staff.

- d) *The applicant has provided on-site parking for car share vehicles via a recorded written agreement between the landowner and the city that runs with the land. Agreement shall provide for proof of a perpetual agreement with a car share agency to provide at least one (1) car share vehicle on-site.*

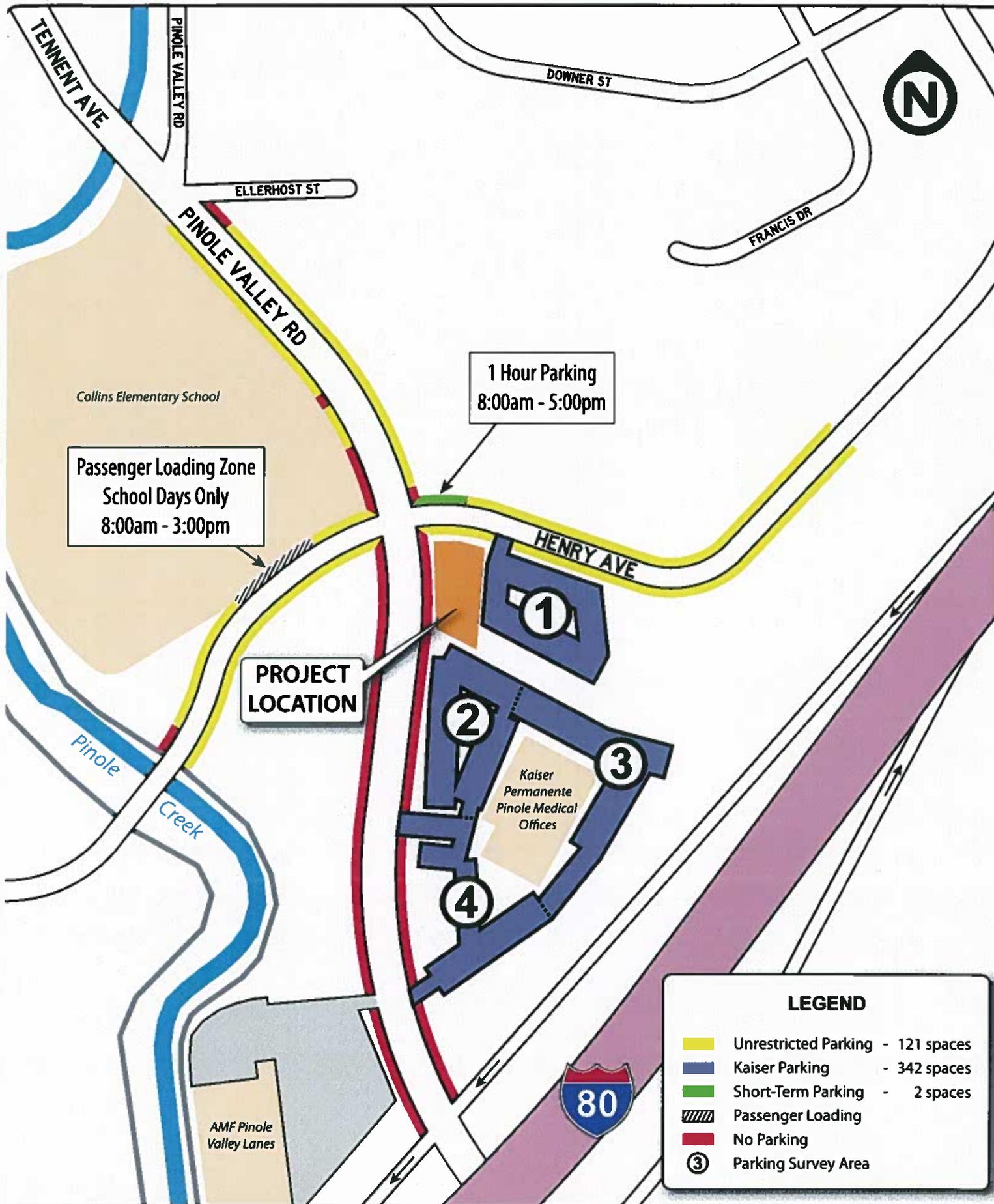
The applicant has proposed to provide a parking space for car sharing so it appears this circumstance would be met, subject to approval of the details of the car share agreement by City Staff.

It should again be noted the parking demand estimates presented in this report do not account for the applicants proposed Travel Demand Management Program. Although it is reasonable to assume that incentives to use transit could reduce the project parking demand, this is normally not included in the preliminary parking demand calculations. This would typically only be included in the calculations if the requirement to provide the transit incentives is officially codified as part of the project approvals so it would guarantee the incentives will continue in

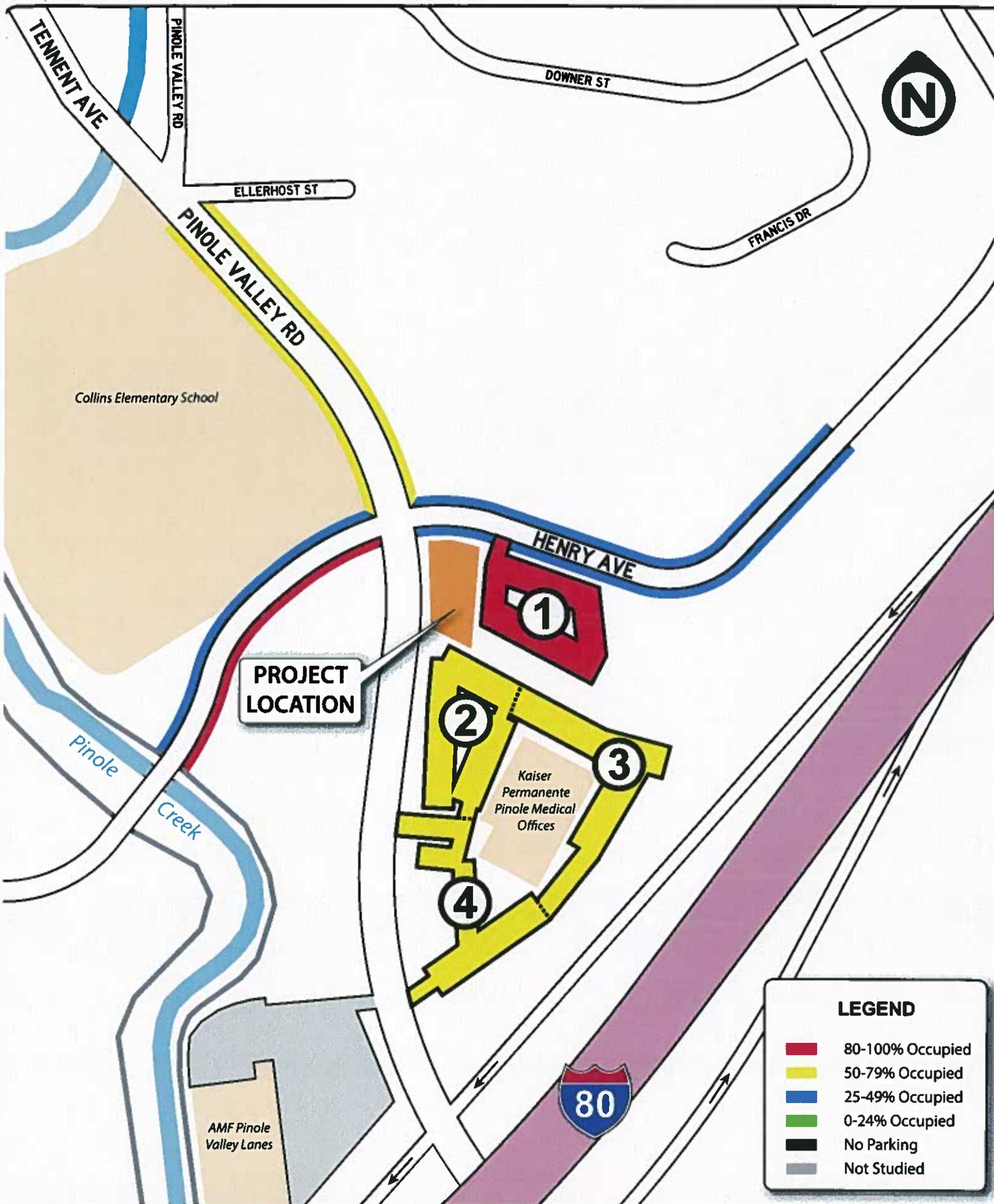
perpetuity. However, as noted above, with financial incentives to use transit the ITE data does indicate the project parking demand could be reduced by as much as 16 percent, which could equate to a reduction to the overall project parking demand of about 6 spaces.

#### **4.4 Pedestrian and Bicycle Impacts**

The proposed project would generate additional pedestrian and bicycle traffic in the area, thereby potentially increasing conflicts between vehicles, bicycles, and pedestrians. However, the project would not conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks) or generate pedestrian, bicycle, or transit travel demand that would not be accommodated by existing transit, bicycle, or pedestrian facilities and plans. Along the perimeter of the project sidewalks are already provided. Therefore, based on the City's significance criteria the project's impacts on pedestrian and bicycle travel would be considered less than significant and no mitigations would be required.



**FIGURE 1 | PARKING INVENTORY**  
**PARKING ANALYSIS**  
**Gateway Medical Building**  
**City of Pinole**



**FIGURE 2 | PARKING SURVEY RESULTS - TUESDAY APRIL 12, 2016**  
**PARKING ANALYSIS**  
 2:00 PM - 3:00 PM

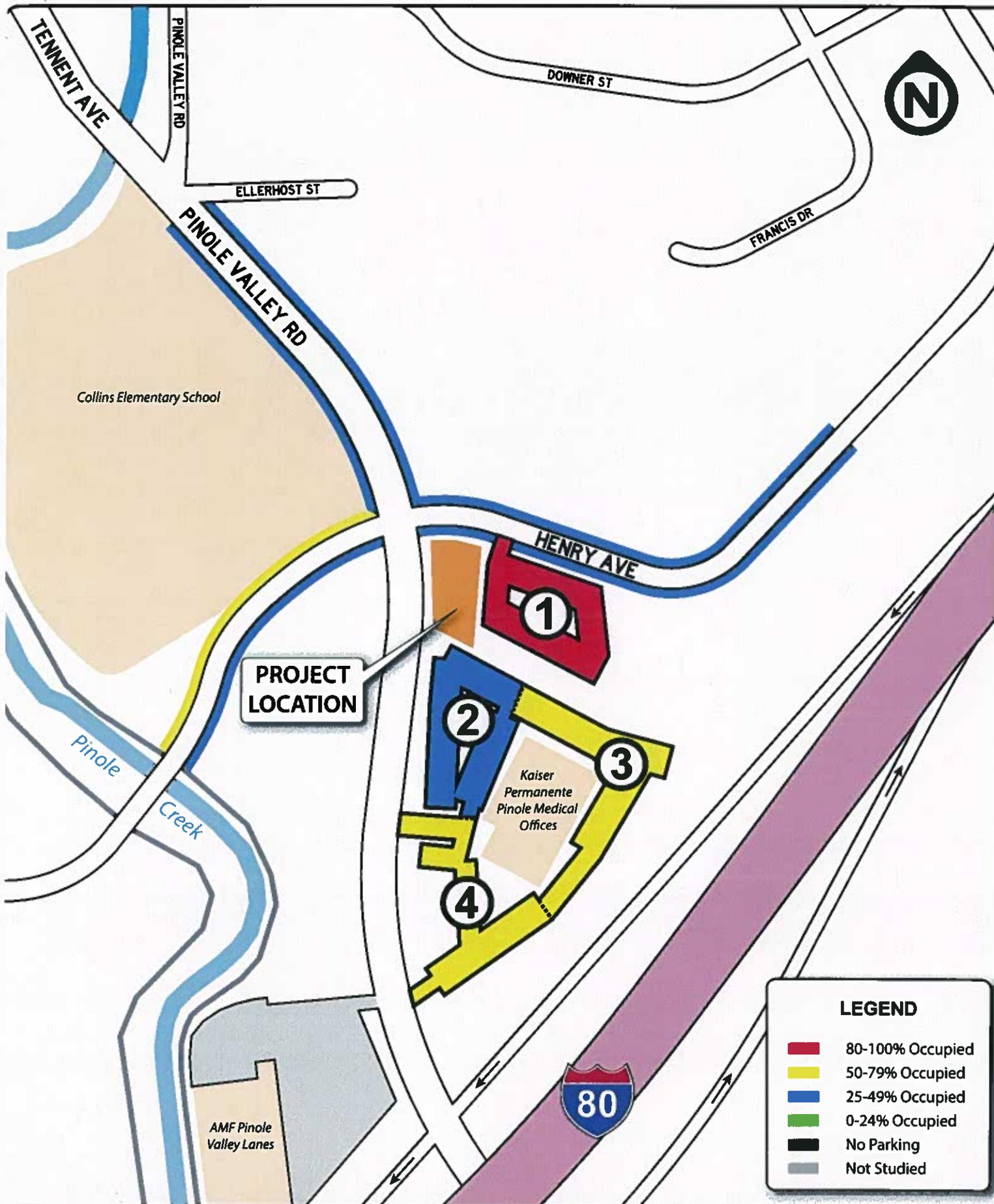
**Gateway Medical Building**  
 City of Pinole



**FIGURE 3 | PARKING SURVEY RESULTS - WEDNESDAY APRIL 13, 2016**  
**PARKING ANALYSIS**

11:00 AM - 12:00 PM

**Gateway Medical Building**  
**City of Pinole**



**FIGURE 4 | PARKING SURVEY RESULTS - THURSDAY APRIL 14, 2016**  
**PARKING ANALYSIS**

2:00 PM - 3:00 PM

**Gateway Medical Building**  
**City of Pinole**

**Table 1**

**Summary of Parking Survey Results for the Peak Period**

Street	Side of Street	Number of Spaces	Survey Day 1 Tues 4/12 2:00 PM - 3:00 pm			Survey Day 2 Wed 4/13 11:00 AM - 12:00 PM			Survey Day 3 Thurs 4/14 2:00 PM - 3:00 PM		
			Parked Vehicles	Occupied	Percent Occupied	Parked Vehicles	Occupied	Percent Occupied	Parked Vehicles	Occupied	Percent Occupied
Pinole Valley Rd	West	22	15	68.2%	8	36.4%	7	31.8%			
Pinole Valley Rd	East	14	7	50.0%	5	35.7%	8	57.1%			
Henry Ave (West of PVR)	North	21	10	47.6%	4	19.0%	18	85.7%			
Henry Ave (West of PVR)	South	16	16	100.0%	8	50.0%	9	56.3%			
Henry Ave (East of PVR)	North	25	12	48.0%	10	40.0%	12	48.0%			
Henry Ave (East of PVR)	South	25	10	40.0%	10	40.0%	13	52.0%			
Kaiser Parking Area	1	105	96	91.4%	92	87.6%	95	90.5%			
Kaiser Parking Area	2	100	53	53.0%	56	56.0%	53	53.0%			
Kaiser Parking Area	3	93	55	59.1%	43	46.2%	70	75.3%			
Kaiser Parking Area	4	44	24	54.5%	27	61.4%	33	75.0%			
<b>Total</b>	<b>N/A</b>	<b>465</b>	<b>298</b>	<b>64.1%</b>	<b>263</b>	<b>56.6%</b>	<b>318</b>	<b>68.4%</b>			

NOTE: PVR = Pinole Valley Rd

**Table 2  
Parking Survey Results for Tuesday 4/12/2016**

Street	Side of Street	Number of Spaces	1:00 PM - 2:00 PM		2:00 PM - 3:00 PM	
			Parked Vehicles	Percent Occupied	Parked Vehicles	Percent Occupied
Pinole Valley Rd	West	22	9	40.9%	15	68.2%
Pinole Valley Rd	East	14	9	64.3%	7	50.0%
Henry Ave (West of PVR)	North	21	11	52.4%	10	47.6%
Henry Ave (West of PVR)	South	16	7	43.8%	16	100.0%
Henry Ave (East of PVR)	North	25	10	40.0%	12	48.0%
Henry Ave (East of PVR)	South	25	12	48.0%	10	40.0%
Kaiser Parking Area	1	105	90	85.7%	96	91.4%
Kaiser Parking Area	2	100	27	27.0%	53	53.0%
Kaiser Parking Area	3	93	19	20.4%	55	59.1%
Kaiser Parking Area	4	44	28	63.6%	24	54.5%
<b>Total</b>	<b>N/A</b>	<b>465</b>	<b>222</b>	<b>47.7%</b>	<b>298</b>	<b>64.1%</b>

NOTE: PVR = Pinole Valley Rd

**Table 3**  
**Parking Survey Results for Wednesday 4/13/2016**

Street	Side of Street	Number of Spaces	10:00 AM - 11:00 AM		11:00 AM - 12:00 PM		12:00 PM - 1:00 PM		1:00 PM - 2:00 PM	
			Parked Vehicles	Percent Occupied	Parked Vehicles	Percent Occupied	Parked Vehicles	Percent Occupied	Parked Vehicles	Percent Occupied
Pinole Valley Rd	West	22	10	45.5%	8	36.4%	9	40.9%	8	36.4%
Pinole Valley Rd	East	14	5	35.7%	5	35.7%	5	35.7%	5	35.7%
Henry Ave (West of PVR)	North	21	6	28.6%	4	19.0%	4	19.0%	5	23.8%
Henry Ave (West of PVR)	South	16	7	43.8%	8	50.0%	8	50.0%	6	37.5%
Henry Ave (East of PVR)	North	25	9	36.0%	10	40.0%	9	36.0%	11	44.0%
Henry Ave (East of PVR)	South	25	10	40.0%	10	40.0%	14	56.0%	12	48.0%
Kaiser Parking Area	1	105	91	86.7%	92	87.6%	92	87.6%	79	75.2%
Kaiser Parking Area	2	100	48	48.0%	56	56.0%	41	41.0%	21	21.0%
Kaiser Parking Area	3	93	30	32.3%	43	46.2%	38	40.9%	14	15.1%
Kaiser Parking Area	4	44	28	63.6%	27	61.4%	24	54.5%	14	31.8%
<b>Total</b>	<b>N/A</b>	<b>465</b>	<b>244</b>	<b>52.5%</b>	<b>263</b>	<b>56.6%</b>	<b>244</b>	<b>52.5%</b>	<b>175</b>	<b>37.6%</b>

Street	Side of Street	Number of Spaces	2:00 PM - 3:00 PM		3:00 PM - 4:00 PM		4:00 PM - 5:00 PM		5:00 PM - 6:00 PM	
			Parked Vehicles	Percent Occupied						
Pinole Valley Rd	West	22	8	36.4%	6	27.3%	4	18.2%	2	9.1%
Pinole Valley Rd	East	14	7	50.0%	5	35.7%	5	35.7%	5	35.7%
Henry Ave (West of PVR)	North	21	5	23.8%	6	28.6%	6	28.6%	5	23.8%
Henry Ave (West of PVR)	South	16	7	43.8%	10	62.5%	9	56.3%	4	25.0%
Henry Ave (East of PVR)	North	25	9	36.0%	9	36.0%	10	40.0%	8	32.0%
Henry Ave (East of PVR)	South	25	12	48.0%	11	44.0%	11	44.0%	9	36.0%
Kaiser Parking Area	1	105	88	83.8%	89	84.8%	84	80.0%	81	77.1%
Kaiser Parking Area	2	100	42	42.0%	37	37.0%	36	36.0%	40	40.0%
Kaiser Parking Area	3	93	30	32.3%	38	40.9%	42	45.2%	36	38.7%
Kaiser Parking Area	4	44	20	45.5%	22	50.0%	19	43.2%	17	38.6%
<b>Total</b>	<b>N/A</b>	<b>465</b>	<b>228</b>	<b>49.0%</b>	<b>233</b>	<b>50.1%</b>	<b>226</b>	<b>48.6%</b>	<b>207</b>	<b>44.5%</b>

NOTE: PVR = Pinole Valley Rd

**Table 4**  
**Parking Survey Results for Thursday 4/14/2016**

Street	Side of Street	Number of Spaces	10:00 AM - 11:00 AM		11:00 AM - 12:00 PM		12:00 PM - 1:00 PM		1:00 PM - 2:00 PM	
			Parked Vehicles	Percent Occupied	Parked Vehicles	Percent Occupied	Parked Vehicles	Percent Occupied	Parked Vehicles	Percent Occupied
Pinole Valley Rd	West	22	5	22.7%	6	27.3%	5	22.7%	6	27.3%
Pinole Valley Rd	East	14	7	50.0%	7	50.0%	8	57.1%	8	57.1%
Henry Ave (West of PVR)	North	21	5	23.8%	4	19.0%	5	23.8%	5	23.8%
Henry Ave (West of PVR)	South	16	6	37.5%	10	62.5%	9	56.3%	7	43.8%
Henry Ave (East of PVR)	North	25	12	48.0%	11	44.0%	14	56.0%	13	52.0%
Henry Ave (East of PVR)	South	25	14	56.0%	15	60.0%	15	60.0%	15	60.0%
Kaiser Parking Area	1	105	93	88.6%	94	89.5%	92	87.6%	69	65.7%
Kaiser Parking Area	2	100	41	41.0%	41	41.0%	38	38.0%	18	18.0%
Kaiser Parking Area	3	93	23	24.7%	23	24.7%	32	34.4%	16	17.2%
Kaiser Parking Area	4	44	39	88.6%	39	88.6%	12	27.3%	11	25.0%
<b>Total</b>	<b>N/A</b>	<b>465</b>	<b>245</b>	<b>52.7%</b>	<b>250</b>	<b>53.8%</b>	<b>230</b>	<b>49.5%</b>	<b>168</b>	<b>36.1%</b>

Street	Side of Street	Number of Spaces	2:00 PM - 3:00 PM		3:00 PM - 4:00 PM		4:00 PM - 5:00 PM		5:00 PM - 6:00 PM	
			Parked Vehicles	Percent Occupied						
Pinole Valley Rd	West	22	7	31.8%	5	22.7%	3	13.6%	1	4.5%
Pinole Valley Rd	East	14	8	57.1%	7	50.0%	7	50.0%	7	50.0%
Henry Ave (West of PVR)	North	21	18	85.7%	7	33.3%	7	33.3%	6	28.6%
Henry Ave (West of PVR)	South	16	9	56.3%	7	43.8%	5	31.3%	4	25.0%
Henry Ave (East of PVR)	North	25	12	48.0%	14	56.0%	14	56.0%	13	52.0%
Henry Ave (East of PVR)	South	25	13	52.0%	11	44.0%	11	44.0%	12	48.0%
Kaiser Parking Area	1	105	95	90.5%	97	92.4%	90	85.7%	84	80.0%
Kaiser Parking Area	2	100	53	53.0%	43	43.0%	57	57.0%	56	56.0%
Kaiser Parking Area	3	93	60	64.5%	33	35.5%	56	60.2%	52	55.9%
Kaiser Parking Area	4	44	32	72.7%	14	31.8%	24	54.5%	18	40.9%
<b>Total</b>	<b>N/A</b>	<b>465</b>	<b>307</b>	<b>66.0%</b>	<b>238</b>	<b>51.2%</b>	<b>274</b>	<b>58.9%</b>	<b>253</b>	<b>54.4%</b>

NOTE: PVR = Pinole Valley Rd

June 2, 2016

City of Pinole  
Planning Department  
Pinole, Ca 94564



Mr. Rhodes,

I am the neighbor across the street from the proposed medical office building on APN 401-211-033, Pinole Valley Road at Henry Ave. I sent a letter showing my concern for the CUP 15-02 for reduced parking as not well balanced. I stated that "what I don't need is an overflow of people parking on the streets in front of my house." After seeing the proposed building plans and reviewing the project I now have a better understanding and do not oppose the project and granting of the CUP 15-02 as it is beneficial to the community and well within the City of Pinole's regulations. This project is properly designed and well balanced and I support the Gateway Medical Office and granting the CUP 15-02.

I also believe that this proposed development is a better fit for our neighborhood than my feared restaurant. A restaurant use would not only generate more traffic but that traffic would continue into the evening. A restaurant would also generate more garbage of which is currently find blowing up my long driveway from Kaiser.

The development of this parcel is inevitable so please stop delaying its approval. I fear that a much less desirable development could replace the good intentions of this current developer.

I met with the project applicant, Dr. Lee, who explained that the parking lot across the street where the Kaiser employees park is actually part of his proposed project. I walked around the Kaiser parking lot and saw that more than a third of the Kaiser parking spaces are not used at all times of the day. If the Kaiser employees and patients park in the Kaiser parking area there will be no lack of parking or overflow on to the street that I was concerned about. Dr Lee also informed me that there will be stripping and signage in his parking area to let people know that only patients and employees are allowed to park there.

The parking problem on Henry Ave is caused more by commuters who leave their cars parked all day in front of our homes. It would be beneficial if the City of Pinole were to implement 4 hour parking on the north side of Henry Avenue. Permit all day parking on the south side for commuters.

Thanks,

Greg Jacobs  
2492 Henry Ave  
Pinole, CA

FYI, my experience of and about this city stems from the following:  
I am a previous member of the city design review board, a former employee, Civil Engineer for 6 years and an elected city clerk. I have significant knowledge of the city. I have lived here for over 40 years.

June 3, 2016

Planning Manager Winston Rhodes  
Pinole Planning Commission  
2131 Pear Street  
Pinole, CA. 94564

Dear Manager Rhodes:

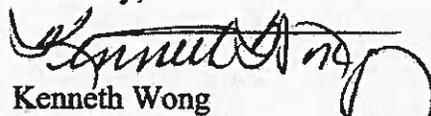
We are residents of Pinole for 30+ years. While we worked in San Francisco for the federal government, the Department of Justice and the Department of Homeland Security, we patronized local Pinole businesses and medical providers.

We both are continuing patients of Dr. Scott Lee, ophthalmology. We understand he is seeking to relocate his Tara Hills office to the Gateway Medical Center on Pinole Valley Road. We are encouraging this Planning Commission to consider his application for office space in the most favorable light possible. In our careers, we dealt extensively with laws, regulations, and protocols. We successfully maneuvered through the bureaucracy to promote the mission of our respective agencies. With this in mind, we are certain considerations and compromises can be exercised by the Planning Commission to ensure that Dr. Lee's long time practice is not forced to leave Pinole.

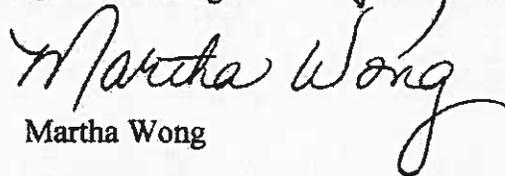
We have a flourishing community and while a goal is to attract more service professionals to the area, Pinole also needs to accommodate the talent already established in this area, such as Dr. Lee. Further, established talent attracts to Pinole other professionals and other patients that only further Pinole's interests.

We thus strongly encourage this Planning Commission to take the necessary steps to secure Dr. Lee's ophthalmology practice at the Gateway Medical Center. We are happy to comment further or to work with the Commission to accomplish this goal.

Sincerely,



Kenneth Wong



Martha Wong

2406 Christian Court  
Pinole, CA. 94564  
(510) 758 1090

## Winston Rhodes

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**From:** Scott Lee [scottle@gmail.com]  
**Sent:** Monday, June 06, 2016 8:11 AM  
**To:** Winston Rhodes  
**Subject:** Fwd:  
**Attachments:** CCF06062016.pdf

Dear Winston,

A few of my patients have said that they can not come to the planning commission meeting next week. They said they would be emailing or delivering letters to you directly. These patients dropped this letter off asking that it be included in the planning commission packet, and I am forwarding it to you.

Many thanks,  
Scott

**PLANNING COMMISSION RESOLUTION 16-05  
WITH EXHIBIT A: CONDITIONS OF APPROVAL**

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PINOLE APPROVING A DESIGN REVIEW REQUEST (DR 15-10) AND APPROVING A CONDITIONAL USE PERMIT REQUEST (CUP 15-02) FOR AN APPROXIMATELY 9,182 SQUARE FOOT MEDICAL OFFICE BUILDING (APN: 401-021-033) LOCATED SOUTHEAST OF THE INTERSECTION OF PINOLE VALLEY ROAD AND HENRY AVENUE**

**WHEREAS**, Agape, LLC (hereinafter referred to as applicant) filed an application with the City of Pinole (hereinafter referred to as City) for a design review request (DR 15-10) and a conditional use permit (CUP 15-02) in order to construct and operate a new 2-story, approximately 9,182-square foot ambulatory surgical center and 31 associated surface automobile parking spaces at the southeast corner of Pinole Valley Road and Henry Avenue (APN: 401-021-033) in accordance with Title 17 of the Pinole Municipal Code; and

**WHEREAS**, the proposed new medical office building and associated parking and related improvements are subject to design review; and

**WHEREAS**, the proposed medical office building conforms to the land use designation of the Pinole General Plan and the Pinole Valley Road portion of the Three Corridors Specific Plan and to the related policies and standards of those respective plans; and

**WHEREAS**, the application includes a request for a reduction in the number of required automobile parking spaces from 37 parking spaces to 31 parking spaces and, pursuant to Title 17 of the Pinole Municipal Code, requires a conditional use permit; and

**WHEREAS**, an Initial Environmental Study was prepared and identified no potential significant adverse environmental effects associated with the proposed project; and

**WHEREAS**, a trip generation and parking analysis study was prepared for the proposed project by the City to evaluate project internal circulation and access as well as the auto parking demand for the proposed project as well as the adequacy of the parking supply; and

**WHEREAS**, the proposed project meets the criteria for a categorical exemption under the California Environmental Quality Act (CEQA) and properly qualifies for an exemption under CEQA Guidelines §15303 and 15332 as both a Class 3 and Class 32 exemption, respectively; and

**WHEREAS**, the Planning Commission of the City of Pinole is the appropriate authority to hear and take action on this project; and

**WHEREAS**, the Planning Commission has held a public workshop on April 25, 2016 to consider the proposed project and obtain public input about the proposed project; and

**WHEREAS**, the Planning Commission has conducted a duly noticed public hearing to consider DR 15-10 and CUP 15-02 on June 13, 2016, and

**WHEREAS**, after the close of a duly noticed public hearing, the Planning Commission considered all public comments received both before and during the public hearing, the presentation by City staff, the staff report, and all other pertinent documents regarding the proposed development project prior to taking action.

**NOW, THEREFORE,** the Planning Commission of the City of Pinole hereby finds, determines, and resolves as follows:

1. The Planning Commission has considered the full record before it, which may include but is not limited to such information as the staff report, testimony by staff and the public, and other materials and evidence submitted or provided to it. Furthermore, the recitals set forth above are true and correct and are incorporated herein by reference.
2. In accordance with the provisions of CEQA, the Planning Commission finds that proposed project is categorically exempt because it is a small structure on a site located in an urban area of the City of Pinole with less than 10,000 square feet of floor area; is consistent with existing Office Professional Mixed Use (OPMU) zoning district as conditioned; does not involve the use of significant amounts of hazardous substances; and is adequately served by all necessary public services and facilities. Therefore, the proposed project meets all requirements established by Section 15030 of the CEQA Guidelines, and is exempt from the provisions of CEQA under Class 3.

Additionally, the Planning Commission finds the proposed project is categorically exempt from CEQA because it is an infill development project consistent with the site's current Service Sub-Area General Plan land use designation and the OPMU zoning district; is located within the limits of the City of Pinole; is approximately 0.6-acres in size; and is surrounded by urban uses and has been heavily disturbed by past grading and partial paving. The site is of no value as habitat for endangered, rare, or threatened species and approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. Existing water and sewer lines are located in surrounding roadways, to which the project would connect for service. The site can also be adequately served by police and fire service providers, both of which are located a short distance to the project site. Therefore, the proposed project meets all qualifications of Section 15332 of the CEQA Guidelines, and is exempt from the provisions of CEQA under Class 32.

Furthermore, no exceptions to categorical exceptions identified in the CEQA Guidelines apply to the proposed project.

3. The proposed project, as conditioned, is consistent with the objectives of the City's General Plan, applicable zoning regulations and applicable provisions of the Three Corridors Specific Plan.
4. The proposed project, as conditioned, will not create conflicts with vehicular, bicycle, or pedestrian transportation circulation modes.
5. The establishment, maintenance, and operation of the proposed medical office building as conditioned will not be detrimental to the health safety, peace, morals, comfort or general welfare of the persons residing or working in the surrounding neighborhood of the project site or general welfare of the City of Pinole.
6. The site of the proposed project, as conditioned, is physically suitable for the type, density and intensity of use and related structures proposed.

7. The proposed project, as conditioned, will not be contrary to the specific intent clauses, development regulations, or performance standards established for the OPMU zoning district in which it is located.
8. The proposed use and related structures, as conditioned, are compatible with other land uses, transportation and service facilities in the project vicinity.
9. The site layout, as well as the landscaping, lighting, and other development features, as conditioned, are compatible with and complement the existing surrounding environment and ultimate character of the area under the General Plan and Three Corridors Specific Plan.
10. Based on the information provided Gateway Medical Center Trip Generation and Parking Analysis dated June 6, 2016 and hereby incorporated by reference; the use, as conditioned, will be adequately served by the proposed parking due to the nature of the proposed operation; proximity to nearby frequent transit service on Pinole Valley Road; transportation characteristics of persons residing, working, or visiting the site; or because the applicant has undertaken a travel demand management program that will reduce parking demand at the site.
11. Parking demand generated by the proposed project will not exceed the capacity of or have a detrimental impact on the supply of on-street parking in the surrounding area.
12. The applicant will provide on-site parking for one car share vehicle via a recorded written agreement between the landowner and the City that runs with the land.
13. Hereby approves Design Review 15-10 and Conditional Use Permit 15-02 as provided in the staff report, and subject to the Conditions of Approval attached as Exhibit A to this Resolution.
14. The approval of Design Review 15-10 and Conditional Use Permit 15-02 shall terminate on June 13, 2016, unless exercised and actual construction or alteration under valid permits has begun within said period or a written request has been submitted to the City, prior to the expiration date, for an extension of time as allowed under the Zoning Ordinance.

The above action is final unless an appeal is filed pursuant to Chapter 17.10 of the Pinole Municipal Code within ten (10) calendar days following Planning Commission action.

**PASSED AND ADOPTED** by the Planning Commission of the City of Pinole on this 13<sup>th</sup> day of June 2016, by the following vote:

AYES:  
NOES:  
ABSTAIN:  
ABSENT:

**David Kurrent, Chair, 2016-2017**

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**ATTEST:**

**Winston Rhodes, AICP, Planning Manager**

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
 DESIGN REVIEW AND CONDITIONAL USE PERMIT  
 EXHIBIT A CONDITIONS OF APPROVAL**



<u>Verification</u> (date and Signature)	<u>Monitoring Department /Division</u>	<u>Timing/ Implementation</u>	
	Development Services	Ongoing	<p>1. If any of these conditions are found to be disregarded, the use permit for the 6 automobile parking space reduction will be subject to revocation pursuant to Section 17.10.120 of the Pinole Municipal Code. If necessary, the Planning Commission may modify the use permit or may revoke the use permit after holding a noticed public hearing and making applicable findings.</p>
	Planning	Ongoing	<p>2. The project shall be constructed and operated in substantial compliance with the project description statement date stamped received June 2, 2016 and the accompanying project plans date stamped received June 2, 2016 and December 2, 2015 and material submitted for the Conditional Use Permit and Design Review request unless modified by the conditions of approval below.</p>

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
 DESIGN REVIEW AND CONDITIONAL USE PERMIT  
 EXHIBIT A CONDITIONS OF APPROVAL**



	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
3.		The proposed use shall operate in a manner consistent with all applicable federal, State, and local regulations.	Development Services, Police, and Fire Departments
4.		All development permit drawings and subsequent construction shall substantially conform to the planning application design information and drawings submitted for Planning Commission consideration and as recommended for approval. Any modifications must be reviewed by the City Engineer and Planning Manager who shall determine whether the modification requires additional approval of the Planning Commission.	Development Services

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
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	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
5.	Ongoing	Planning	
6.	Ongoing	Development Services Department and Police Department	

The Applicant shall hold harmless the City, its Council Members, its Planning Commission, officers, agents, employees, and representatives from liability for any award, damages, costs and fees incurred by the City and/or awarded to any plaintiff in an action challenging the validity of this permit or any environmental or other documentation related to approval of this permit. Applicant further agrees to provide a defense for the City in any such action.

The regular operating hours of the Gateway Medical Center shall be 7:00AM – 7:00PM Monday through Friday. Limited Saturday and Sunday operations shall be allowed only to accommodate medical emergencies.

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
 DESIGN REVIEW AND CONDITIONAL USE PERMIT  
 EXHIBIT A CONDITIONS OF APPROVAL**



	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
7.	Ongoing	Planning	
	All building permit drawings and subsequent construction shall substantially conform to the approved planning application drawings. The Planning Manager shall determine whether the modification requires additional approval of the Planning Commission or City Council.		
8.	Ongoing	Development Services Department	
	The project site and surrounding area shall be maintained in a clean and orderly manner at all times. Landscaping material shall be regularly maintained around the site so that it cannot be used as a hiding place or interfere with safe ingress or egress to the project site. All ground cover or shrubs adjacent to the project driveway should be trimmed to be no higher than 2 feet and any trees should be limbed up to at least 6 feet. On-site lighting shall be maintained in good working condition at all times.		
9.	Ongoing	Development Services Department	
	A copy of these permit conditions of approval shall be kept on premises and made available upon request.		

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
 DESIGN REVIEW AND CONDITIONAL USE PERMIT  
 EXHIBIT A CONDITIONS OF APPROVAL**



	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
10.	<p>If the operation of the use results in conflicts pertaining to parking, noise, hazardous materials, nuisance, traffic or other impacts, at the discretion of staff, the use permit may be referred to the Planning Commission for subsequent review and potential revocation at a public hearing.</p>	Development Services Department	
11.	<p>All facility employees shall be required by the operator to park in the least convenient available spaces to maximize efficient available parking for customers.</p>	Development Services Department	

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
 DESIGN REVIEW AND CONDITIONAL USE PERMIT  
 EXHIBIT A CONDITIONS OF APPROVAL**



	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
12.		Ongoing	Development Services Department
	The applicant or medical center operator shall make rideshare and public transit information continuously available on premises to employees and customers.		
13.		Ongoing	Development Services Department
	The applicant shall offer free transit vouchers to all medical center employees with 100 percent subsidy to encourage and incentivize travel to the medical center by available transit options rather than by automobile. The number of transit vouchers actually provided to employees at any given time along with supporting documentation from transit providers shall be made available to the City by the applicant or medical center operator upon request.		

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
 DESIGN REVIEW AND CONDITIONAL USE PERMIT  
 EXHIBIT A CONDITIONS OF APPROVAL**



	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
14.	Ongoing	Development Services Department	
15.	Ongoing	Development Services Department	

The applicant or medical center operator shall provide secure indoor bicycle parking for at least 5 bicycles in the first floor storage room adjacent to the lobby or alternate interior location subject City review and approval. The applicant or medical center operator shall provide outdoor parking for at least 4 bicycles near the building entrance. The bicycle parking rack design shall be included in the building construction plans for City Review and approval.

The applicant or medical center operator shall ensure that the food and beverage area provides service only for medical center employees, patients, and individuals accompanying patients who are receiving treatment. Signs shall be posted at all times informing building users that food and beverage service is limited to the individuals providing or receiving medical treatment on site.

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
DESIGN REVIEW AND CONDITIONAL USE PERMIT  
EXHIBIT A CONDITIONS OF APPROVAL**



	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
<p>16. The applicant shall provide one on-site automobile parking space for car share vehicles on the project site at all times via a recorded written agreement between the landowner and the City that runs with the land. The agreement shall allow for various car share providers over time. The agreement shall be submitted to the City Attorney and Development Services Department for review and approval prior to issuance of a building permit. Once approved by the City Attorney, the applicant shall record the agreement and provide a copy to the Development Services Department prior to building occupancy.</p>	Ongoing	City Attorney and Development Services Department	
<p>17. Failure to obtain prior approval to modify the approved plans may result in having to pay double the original planning application permit fee and/or withholding of the final inspection until such time as the modification(s) to the plans has been reviewed and approved by the Planning Manager or the Planning Commission.</p>	Ongoing	Planning	

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
DESIGN REVIEW AND CONDITIONAL USE PERMIT  
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	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
<p>18. Proposed modification of the operating hours or other aspects of the business, including but not limited to service expansion or intensification of medical services provided that they affect demand for automobile parking on site shall be submitted in writing for review and approval by City staff. Staff will determine if changes are substantial and require further review by the Planning Commission.</p>	<p>Ongoing</p>	<p>Development Services Department</p>	
<p>19. All exterior materials and colors are to be consistent with approved project color / material boards. Once installed, all improvements are to be maintained in accordance with the approved plans. Any changes which materially affect the exterior character shall be resubmitted to the Development Services Department for review and approval. Minor changes may be approved by the Planning Manager. Any changes determined by the Development Services Department to be non-minor may be referred to the Planning Commission after conferring with the Chair of the Planning Commission.</p>	<p>Ongoing</p>	<p>Development Services</p>	

**RESOLUTION 16-05 GATEWAY MEDICAL CENTER  
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		<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
20.	The applicant or medical center operator shall provide full cooperation to the City Police Department.	Ongoing	Police	
21.	All project signage shall be developed in accordance with existing Sign Ordinance (Chapter 17.52 of the Pinole Municipal Code).	Ongoing	Development Services	
22.	<b>BUSINESS EMERGENCY INFORMATION FORM</b> – The property owner shall complete a Police Department “Business Emergency Information Form” and shall deliver it to the Police Department for review and approval prior to project completion.	Ongoing	Police	
23.	The existing historic Faria family plaque shall remain on the project site and maintained in its current condition by the property owner.	Ongoing	Development Services	
24.	<b>GRAFFITI AND VANDALISM CONTROL</b> The property owner and operator shall keep the site clear of graffiti and repair any vandalism damage on a regular and continuous basis. Graffiti-resistant materials shall be used when feasible. All graffiti	Ongoing	Development Services	

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	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
	shall be removed within 72 hours.		
25.	Ongoing	Development Services	
26.	Prior to Issuance of Building Permits	Development Services	
27.	Prior to approval of Grading and Improvement Plans	Development Services	

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	<p>made at least 48 hours in advance. The project applicant shall be responsible for providing a backhoe and operator, as well as a surveyor if needed. All construction plans for activities within pipeline easements shall be submitted to Kinder Morgan Energy Partners, L.P., SFPP L.P., and TOSCO Refining to allow for review prior to commencing work within the easement.</p> <p>After determining the accurate depths and alignments of the pipelines, the project applicant shall further coordinate with Kinder Morgan Energy Partners, L.P., SFPP L.P., TOSCO Refining, and the City Engineer regarding all work that could affect the pipelines in order to ensure compliance with the agreed-upon development restrictions and regulations, which could include, but would not necessarily be limited to, the following:</p> <ul style="list-style-type: none"> <li>• Prohibit deep-rooted trees and structures within pipeline easements;</li> <li>• All excavations within 24-inches of the pipelines shall be accomplished using hand tools only;</li> <li>• Restrict use of heavy vibratory equipment over pipelines; and</li> <li>• Notify Underground Service Alert (USA) at 800-227-2600 at least 48 hours prior to any excavation work.</li> </ul> <p>The agreed-upon development restrictions shall be written up and</p>		

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		<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
	submitted to the City Engineer and the pipeline operators for approval prior to initiation of any on-site construction activities. Once approved, the restrictions shall be noted on the applicable construction plans prior to issuance of any building permit.			
28.	<b>CONSTRUCTION STAGING PLAN</b> – The owner and applicant shall submit construction staging plan for review and approval by the Development Services Department. The construction staging plan shall ensure ongoing vehicle access for Kaiser Permanente	Prior to Issuance of Building Permits	Development Services	
29.	<b>LANDSCAPE PLAN</b> – The owner and applicant shall submit a detailed landscaping plan for review and approval. The landscaping plan shall identify the plant types and sizes, as well as irrigation details. The plan shall indicate the size and species of the two existing trees proposed for removal and satisfy City tree replacement requirements for any protected tree on the project site. The landscape plan shall be subject to review and approval by the Development Services Department.	Prior to Issuance of Any Building Permit	Development Services	
30.	<b>GRADING PLAN</b> – The owner and applicant shall submit a final grading plan for review and approval by the City Engineer.	Prior to Issuance of Any Permit	Development Services Department	

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	<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
31.	DRAINAGE PLAN-The applicant shall submit a final drainage plan to the satisfaction of the City Engineer as part of the building construction plans.	Development Services	
32.	PARKING LOT SIGNING AND STRIPING PLAN – The applicant shall prepare a precise parking lot signage and striping plan for review and approval by the Development Services Department. The plan shall include signage specifying that tandem spaces are for employees only; shall identify a 90-minute time limits for the most convenient parking spaces closest to the building main entrance; shall identify one space for short-term passenger pick up and drop off only; and shall identify the location of one permanent car share space.	Development Services	
33.	LIGHTING PLAN – The applicant shall prepare a final lighting plan for the project including a photometric analysis of the project site and shall ensure all portions of the existing parking lot satisfy City lighting standards including maximum heights. The lighting plan shall include photometric details, light fixture specifications and cut sheets and shall be submitted for review and approval by the Planning Manager and Police Department. Additionally, the lighting plan shall include motion sensitive lighting to illuminate the trash enclosure area and building rear areas to help prevent criminal activity. Exterior lighting is required over all existing building doors and along onsite sidewalk areas.	Police and Development Services	

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34.	<p>All project conditions of approval as well as the timing of the conditions of approval shall be listed in the project construction plans to help ensure that conditions imposed by the City are completed at the appropriate time in the development process.</p>	Development Services	
35.	<p>The project construction plans shall reflect the changes or notations listed below.</p> <ul style="list-style-type: none"> <li>a. The proposed outdoor patio are shall be reconfigured to ensure that it is not located within the existing petroleum pipeline easement area.</li> <li>b. The maximum building height at top of parapet shall be 30 feet.</li> </ul>	Planning	

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		<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
36.	The holder of the use permit shall prepare a security plan for review and approval by the Police Department to help prevent crime on-site during construction and during operations. The security plan shall include emergency contact information, construction staging area information, temporary fencing, construction site addressing, and crime prevention measures.	Prior to Issuance of Building Permit	Development Services and Police	
37.	PERMITS, BONDS, AND INSURANCE - The applicant shall obtain an encroachment permit, posting the required bonds and insurance, for all work to be done in the City's right-of-way. This encroachment permit, if applicable, shall be obtained prior to the issuance of a building permit and prior to any work being done in the City's right-of-way.	Prior to Issuance of Building Permits	Development Services Department	
38.	PLAN CHECK FEES - A plan check fee shall be paid to the Building Division at time of submission of plans as per Section 15.02.060 of the Municipal Code.	Prior to Issuance of a Building Permit	Development Services Department	

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39.	SCHOOL, GROWTH AND PARK IMPACT FEES - Prior to issuance of a building permit, the applicant shall pay all applicable school, growth (as per Chapter 3.20 of the Municipal Code) and park (as per Chapter 16.28 of the Municipal Code) impact fees.	Prior to Issuance of a Building Permit	Development Services Department	
40.	SUBREGIONAL TRANSPORTATION MITIGATION PROGRAM (STMP) FEE - Prior to issuance of a building permit for new construction the applicant shall pay the applicable STMP fee, as per Chapter 16.30 of the Municipal Code.	Prior to Issuance of a Building Permit	Development Services Department	
41.	DEPOSITS TO THE DEVELOPMENT SERVICES DEPARTMENT - The applicant is responsible for all engineering review and inspection and survey services that may be required.	Prior to Issuance of Any Construction Permit	Development Services Department	

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42.	INCLUSION INTO THE LANDSCAPE AND LIGHTING DISTRICT - The project site shall be included within the Landscape and Lighting District Number One. Prior to issuance of any construction permit, the applicant shall agree to be included within the existing Landscape and Lighting District Number One as directed by the City Engineer.	Development Services Department	
43.	<p>PERMITS REQUIRED BY OTHER AGENCIES - The applicant shall obtain all permits and pay all applicable fees that may be required by one or more of the public service or utility providers including, but not limited to, State Department of Fish and Wildlife, Caltrans, Western Contra Costa Transit Authority, WCCUSD, EBMUD, PG&amp;E, Contra Costa County Flood Control and Water Conservation District, and Contra Costa Environmental Health as applicable. If a project component is within the jurisdiction of any of these agencies, verification of permit or waiver of permit must be given to the Development Services Department prior to issuance of any City permits.</p> <p>A Notice of Intent must be issued by the Regional Water Quality Control Board before any construction permit can be issued and a Notice of Termination must be issued at the end of the project by the Regional Water Quality Control Board before a Certificate of Occupancy is issued.</p>	Development Services Department	

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44.	MATERIAL HAULING - The applicant shall submit a proposed material hauling route and schedule as required by the City Engineer. The City Engineer prior to issuance of a building or site development permit shall approve said submittal. All material hauling activities including but not limited to, adherence to approved route, hours of operation, dust control and street maintenance shall be the responsibility of the applicant (as per Section 15.36.080 of the Municipal Code). Violation of such may be cause for suspension of work.	Prior to Issuance of Building Permits	Development Services Department	
45.	CONTRACTORS - Contractors must identify all subcontractors prior to issuance of a building permit. Each subcontractor must obtain a City business license prior to issuance of a building permit or commencing work as per Section 5.04.020 of the Municipal Code.	Prior to Issuance of a Building Permit	Development Services Department	

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	<u>Verification</u> (date and Signature)	<u>Monitoring Department /Division</u>	<u>Timing/ Implementation</u>	
				<p><b>During Construction</b></p> <p><b>46. CONSTRUCTION NUISANCE PREVENTION</b> - The following provisions shall be followed during all construction activities for the project:</p> <p>A. Construction activities are restricted to between 7:00A.M. and 5:00 P.M., Monday through Friday and 9:00 A.M. to 6:00 P.M. on Saturday unless a modification of construction hours is requested and granted by the City as allowed under Chapter 15.02 of the City Municipal Code. Approved construction hours shall be prominently posted on the project site.</p> <p>B. Prior to any earth hauling operations the applicant shall submit a hauling plan for approval by the City Engineer</p> <p>C. Construction-related traffic on Henry Avenue shall be prohibited during the hours of 7:00 – 9:00 A.M. and 2:00 – 4:00 P.M. when Collins Elementary School is in session.</p> <p>D. All construction vehicles shall be properly maintained and equipped with exhaust mufflers and meet State and Federal standards.</p> <p>E. Newly disturbed soil surfaces shall be watered down regularly by a</p>

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47.	<p>water truck maintained on site during all day light hours. and construction grading activity shall be discontinued in wind conditions greater than 10 miles per hour.</p> <p>F. All excavated materials and construction debris shall be covered with a tarp during transit to and from the site. The site shall be cleaned on a daily basis and construction material shall be screened from view whenever feasible.</p> <p>Active construction sites shall be fenced and all equipment and materials shall be secured consistent with an approved Security Plan</p>	Development Services	
48.	<p>DEBRIS BOX - The Applicant shall ensure that prior to commencing construction a contractor shall place on-site a minimum 10 cubic yard "debris box" for receiving and holding of all construction debris.</p> <p>TREE PROTECTION - The applicant shall protect all major vegetation not indicated to be removed or demolished from damage during construction. Tree protection shall comply with the arborist's report and shall include the following measures:</p> <p>A. Protection fencing shall be installed along creek tributary around established trees to remain.</p>	Development Services	

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			<p>B. Oil, gas, chemical, or other hazardous construction material shall not be stored within the drip line of trees to remain.</p> <p>C. Signs, wires, or other types of obstructions shall not be attached to trees, and</p> <p>D. Trenching under the drip line of trees is to be avoided if possible. If trenching is necessary, trenches are to be hand dug and major roots retained.</p>
	Development Services	During Construction	<p>49. CONSTRUCTION MANAGEMENT - The storing of goods and materials on the sidewalk and/or street will not be allowed unless a special permit is issued. The project site shall be kept free of litter and all construction equipment and materials will be secured at the end of each construction day. The applicant's representative in charge shall be at the job site during all working hours. The public streets adjacent to construction activity shall be maintained in a clean and orderly condition to the satisfaction of the City Engineer.</p>

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50.	<p>INSPECTIONS - The applicant shall notify the Development Services Department at least forty-eight (48) hours prior to starting any work pertaining to on-site drainage facilities, grading, or paving, as well as all any work in the City's right-of-way as per Section 15.36.230 of the Municipal Code.</p> <p>The applicant shall arrange all inspections with the Building Division, Fire Department, and Public Works Division. All Building Division inspection requests shall be made at least 24 hours in advance.</p>	During Construction	Development Services	
51.	The construction site shall be cleaned of garbage and debris on a daily basis and maintained in an orderly fashion. All construction equipment shall be secured at the end of each day of construction.	During Construction	Development Services	
	<b>Prior to Occupancy</b>			
52.	<b>OCCUPANCY PERMITS</b> - Occupancy permits shall not be granted until all construction is completed and finalized in accordance with the approved plans and conditions of approval required by the City, or a bond has been posted to cover all costs of the unfinished work as	Prior to Building Occupancy	Development Services Department	

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		<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
	agreed to by the Development Services Department.			
53.	<b>CONSTRUCTION WASTE MANAGEMENT REPORT - DEBRIS REMOVAL</b> - The applicant shall complete post-construction waste management report for review and approval by the Development Services Department prior to final inspection to satisfy CALGreen Building Code Requirements. All building debris shall be properly disposed of outside the City of Pinole.	Prior to Building Occupancy	Development Services Department	
54.	<b>FIRE EXTINGUISHING SYSTEM</b> - Automatic fire extinguishing / sprinkler systems shall be required for the medical center building.	Prior to Building Occupancy	Development Services and Fire Departments	
55.	<b>TREE REPLACEMENT</b> - All trees that are removed from the site shall be replaced as determined by the Development Services Dept. and in accordance with the replacement table for existing trees. Any tree, which is removed, that was shown to be preserved on the final landscaping plan shall be replaced at twice the rate indicated on the replacement table.	Prior to Building Occupancy	Development Services Department	

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	<u>Monitoring Department /Division</u>	<u>Timing/ Implementation</u>	<u>Verification (date and Signature)</u>															
<p><u>STANDARD REPLACEMENT TABLE FOR EXISTING TREES</u></p> <table border="1" data-bbox="760 1108 927 1780"> <thead> <tr> <th>Size of Tree to be removed (Trunk Diameter)</th> <th>36 Inch Box</th> <th>24 Inch Box</th> <th>15 Gallon</th> </tr> </thead> <tbody> <tr> <td>2" or less</td> <td></td> <td></td> <td>1</td> </tr> <tr> <td>4"</td> <td></td> <td>1</td> <td></td> </tr> <tr> <td>6" or greater</td> <td>1</td> <td></td> <td></td> </tr> </tbody> </table> <p>Numbers in replacement table refers to quantities to be used to replace each tree. Each tree container size number in the vertical column represents a 100% replacement value for an existing tree removed (size as noted). These numbers (columns) may be mixed as long as proportionate totals will equal 100%. For example: one 24" DBH tree may be replaced with twelve 15 gallon trees or six 24" box trees or three 24" box trees plus six 15-gallon trees or four 36" box trees, etc.</p>	Size of Tree to be removed (Trunk Diameter)	36 Inch Box	24 Inch Box	15 Gallon	2" or less			1	4"		1		6" or greater	1				
Size of Tree to be removed (Trunk Diameter)	36 Inch Box	24 Inch Box	15 Gallon															
2" or less			1															
4"		1																
6" or greater	1																	

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56.	The applicant shall submit proposed project signage for review and approval by City staff prior to occupancy.	Prior to Building Occupancy	Development Services	
57.	<b>MECHANICAL EQUIPMENT</b> - All mechanical devices and their component parts, such as air conditioners, evaporative coolers, exhaust fans, or similar equipment located wholly or partially on the roof or wall shall be fully screened from view. All wall mounted heating units or air conditioners shall be flush-mounted.	Prior to Building Occupancy	Development Services Department	
58.	<b>DOORS AND ROOF HATCHES</b> - All exterior doors (including storage and utility room doors) that exit to the outside, to a common hallway or separate units shall be of solid core wood or metal construction.  Sliding and storefront doors may be of glass construction if the glass is designed to be laminated safety glass or equivalent as allowed under the Building Code.  All doors described above shall be equipped with the following locking mechanisms:	Prior to Building Occupancy	Development Services and Police Department	

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<u>Verification</u> (date and Signature)	<u>Monitoring</u> Department <u>/Division</u>	<u>Timing/</u> <u>Implementation</u>	
			<p>1. Dead Bolt Locks:</p> <ul style="list-style-type: none"> <li>a. A minimum of 3/4 inch diameter by 1-inch throw.</li> <li>b. High security case hardened bezel around the locking mechanism.</li> <li>c. Made of case hardened steel.</li> <li>d. High security striker plate (4-screw type, screws a minimum of 1 1/2 inches long).</li> <li>e. Where there is glass in a door that would allow a person to operate lock from the outside if the glass were broken a double cylinder dead bolt is required. (Commercial only. Must meet fire Code requirements).</li> </ul> <p>2. All double doors shall have a dead bolt lock, as described in B. 1, in the primary door. The secondary door shall have lock mechanisms that secure the top of the door to the door head and the bottom of the door to the floor. Where the double doors are separated by a mullion both doors shall have dead bolts as described in B-1.</p>

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<p>On doors with locks which open outward, all hinges shall be pinned or have hinges of a type that prevents the hinge pins from being removed from the outside.</p> <p>All entry doors, except those with glass, shall be equipped with a door viewer that has the capabilities of viewing a minimum of 180 degrees.</p> <p>Fire blocks shall be placed at a minimum of two (2) studs spaces on each side of an exterior door, where locks are required, at the same height as the locks. The design and materials used shall aide in preventing the doorjamb from spreading when a pry tool is used to separate the door from the doorjamb at the lock.</p> <p>All roof hatches (access to roof) shall be securely locked from the inside. Dead bolt or similar locking mechanism is recommended.</p>			
59.	Prior to Final Inspection	Development Services	STORMWATER CONTROL OPERATION AND MAINTENANCE - The applicant and owner shall record an operation and maintenance agreement for installed stormwater control improvements to the satisfaction of the City Engineer, if applicable.

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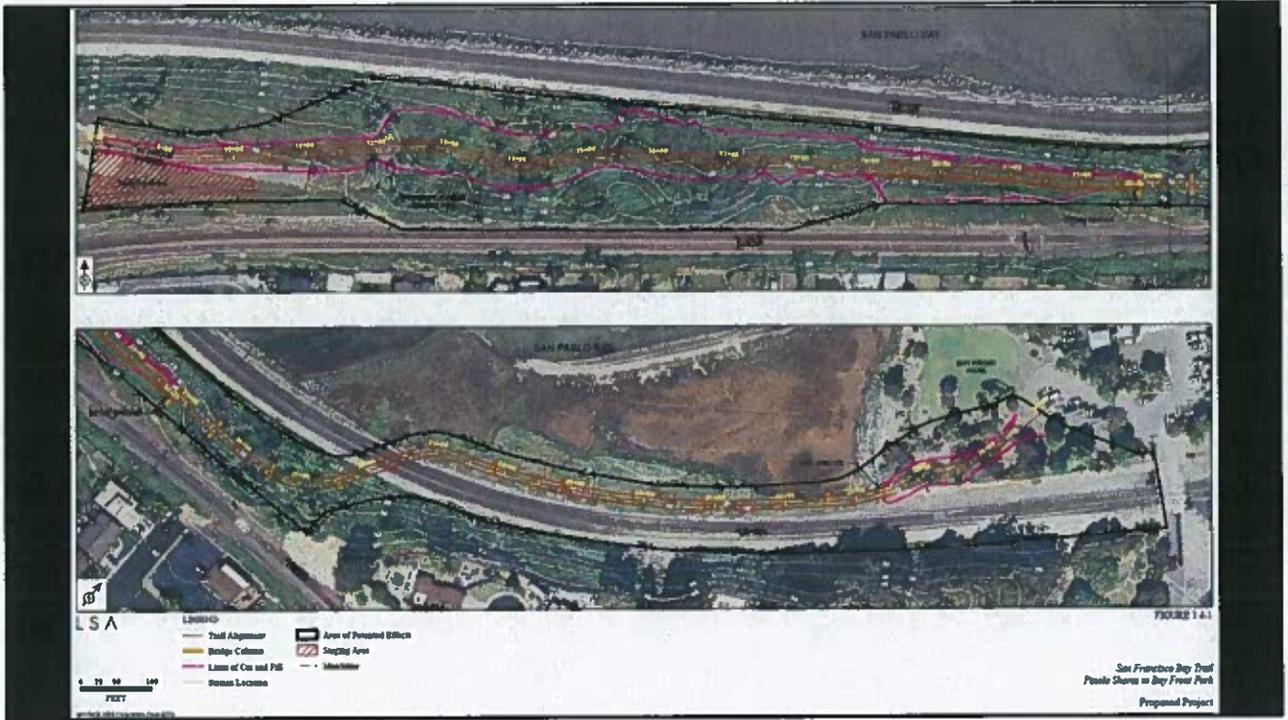
		<u>Timing/ Implementation</u>	<u>Monitoring Department /Division</u>	<u>Verification (date and Signature)</u>
60.	Building and Fire Code Compliance - The applicant shall ensure that all applicable Building and Fire Code requirements are satisfied.	Prior to Final Inspection	Development Services and Fire Department	
61.	ADDRESSING - Prior to final building inspection, permanent illuminated site address shall be installed in compliance with Title 15 of the Municipal Code.	Prior to Final Inspection	Building Division and Fire Department	
62.	SIDEWALK, CURB AND GUTTER REPAIR - The applicant shall repair and replace to existing City standards, any sidewalk, curb and gutter that is damaged along the project site street frontage now or during construction of this project.	Prior to Final Inspection	Development Services	
63.	The facility shall be reviewed for consistency with the approved building plans and these conditions of approval by the Planning Manager and affected City staff.	Prior to Final Inspection	Planning	

# San Francisco Bay Trail: Pinole Shores to Bayfront Park Design Review and Easement Dedication



## Regional Connections

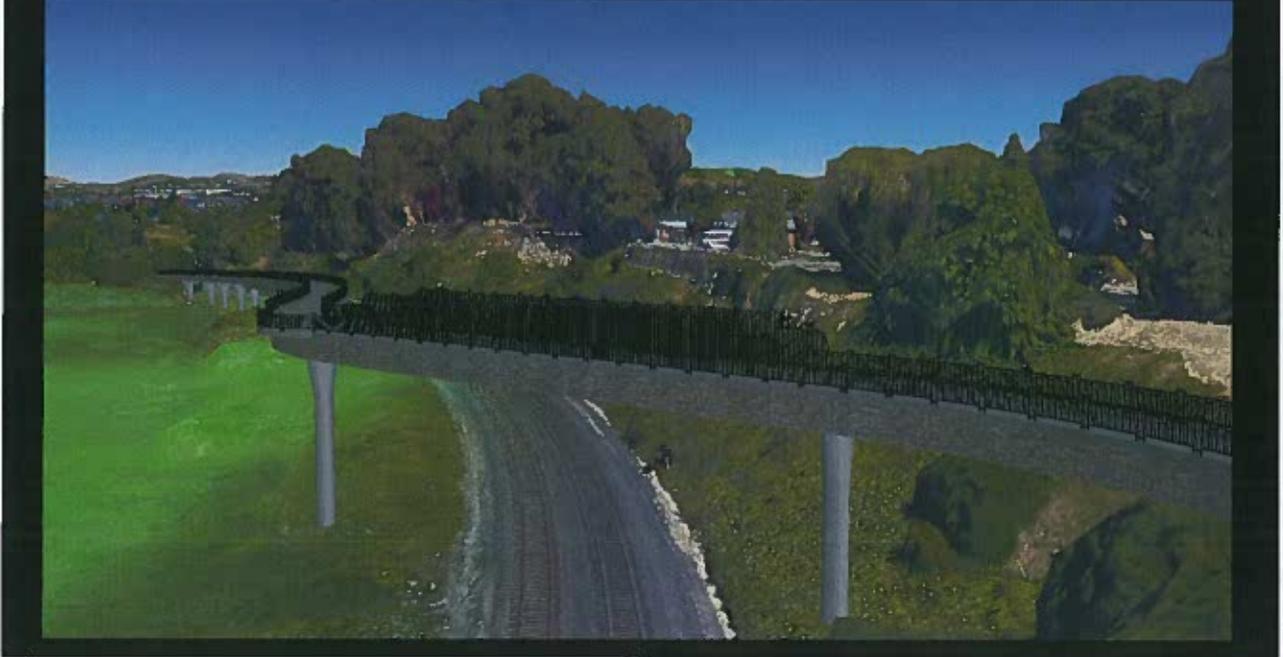




Bridge Rendering – Viewing Northeast



Bridge Rendering – Viewing Northeast



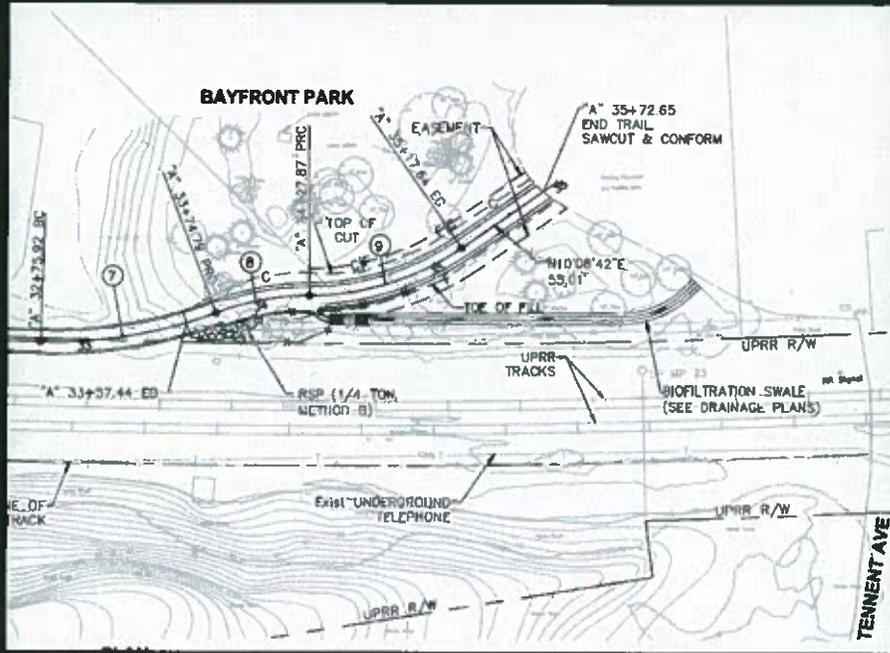
Bridge Rendering – Viewing Southeast



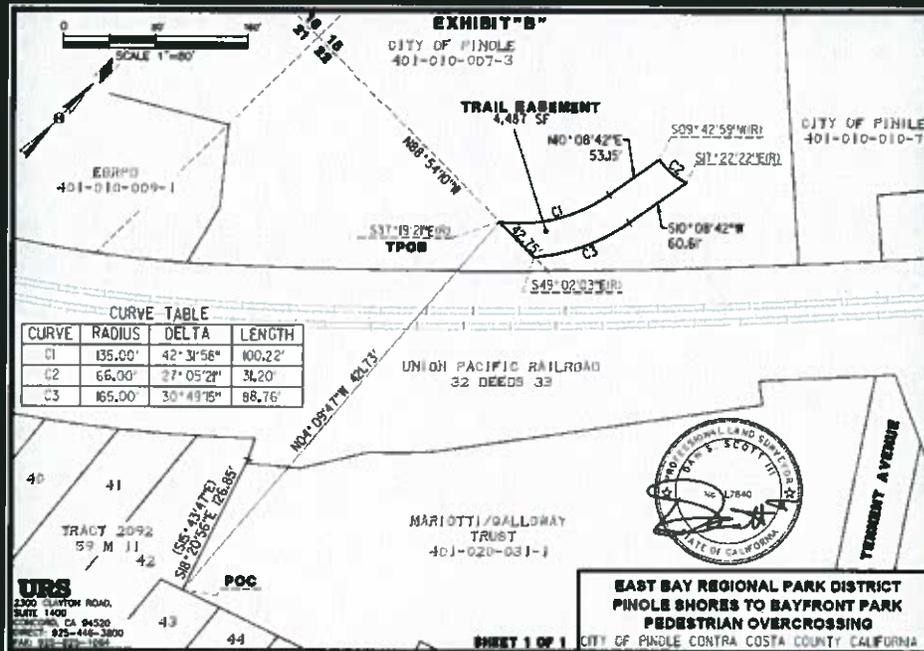
# Bridge Rendering – Viewing East

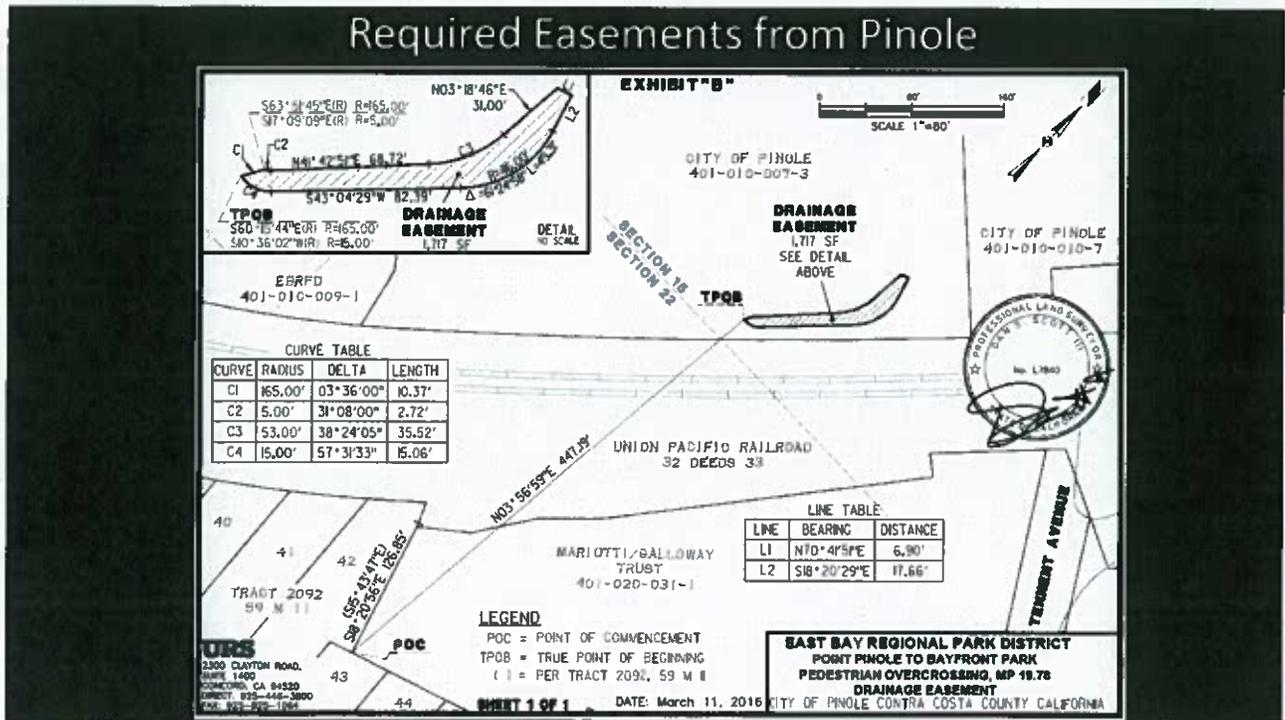


### Bayfront Park Landing Detail



### Required Easements from Pinole





## Project History

**Early – Mid 1970's** - Pinole Hercules Joint Powers Authority established and began planning for a Shoreline Trail from Pt. Pinole to Rodeo

**Mid – 1970's** – San Pablo Bay Shoreline Trail Segments identified in the East Bay Regional Park District Master Plan and the Bay Conservation Development Commission's (BCDC) San Francisco Bay Plan

**2003** – URS submits a 'Design Technical Memorandum' that analyzed several alternatives further and identified the preferred alternative 2A

**2008** – WCCTAC awards \$500,000 of STMP 2006 funds to EBRPD for Environmental and Geotechnical Studies and Preliminary Design, and cost estimates

**August 26<sup>th</sup>, 2010** – Public scoping session to collect comments on the EIR

**December 2010** – TIGER II Grant funds awarded to EBRPD; \$1,581,000 of Measure AA funds transferred to this project subsequent to that award

## Project History

2011 - Preliminary Design Complete

August 23, 2011 – EIR Public Review Hearing

January 2012 – EIR Certified

June 2012 – NEPA CE Completed

June 2012 – April 2016 - UPRR Delays

May 26, 2016 – CPUC Approval

August - City of Pinole Approval

Pending - Final UPRR Design Approval



## Funding Summary

Source	Amount	Funding Status
ATP	\$4,000,000	Secured
EBRPD	\$3,095,000	Secured
WCCTAC STMP	\$500,000	Secured
TIGER II	\$449,000	Secured
PCA	\$119,000	Secured
CA Recreational Trails Grant	\$1,300,000	Secured
<b>Total Secured</b>	<b>\$9,463,000</b>	
<b>Total Project Cost</b>	<b>\$9,463,000</b>	<b>Fully Funded</b>

## Schedule

### Milestones

- Final Design – Summer 2016
- UPRR C & M – Summer 2016
- ROW Certification – Summer 2016
- Out to Bid – Fall 2016
- Construction - Spring 2017



## Questions



